

For Office Use Only:	28TPL2021067 & ZNPL2021064	Public Notice Sign	_____
File Number	_____	Application Fee	_____
Related File Number	_____	Conservation Authority Fee	_____
Pre-consultation Meeting	December 16, 2020	Well & Septic Info Provided	_____
Application Submitted	February 26, 2020	Planner	Nicole Goodbrand
Complete Application	March 25, 2020		

Check the type of planning application(s) you are submitting.

- ☐ Official Plan Amendment
- ☒ Zoning By-Law Amendment
- ☐ Temporary Use By-law
- ☒ Draft Plan of Subdivision/Vacant Land Condominium
- ☐ Condominium Exemption
- ☐ Site Plan Application
- ☐ Consent/Severance
- ☐ Minor Variance
- ☐ Easement/Right-of-Way
- ☐ Extension of a Temporary Use By-law
- ☐ Part Lot Control
- ☐ Cash-in-Lieu of Parking
- ☐ Renewable Energy Project or Radio Communication Tower

Please explain the desired end result of this application (for example: a special zoning provision on the subject lands, changing the zone and/or official plan designation of the subject lands, creating a certain number of lots, or similar)

The proposal is to create six (6) semi-detached lots within a plan of subdivision.

Property Assessment Roll Number: 33104010130290000000



A. Applicant Information

Name of Owner Mayberry Homes - Mike Quattrociocchi

It is the responsibility of the owner or applicant to notify the planner of any changes in ownership within 30 days of such a change.

Address 32 Dunsdon St,

Town and Postal Code Brantford, ON N3R 3J3

Phone Number 519 755 0909

Cell Number _____

Email Mike Quattrociocchi <mquattrociocchi@rogers.com>

Name of Applicant Same as Owner

Address _____

Town and Postal Code _____

Phone Number _____

Cell Number _____

Email _____

Name of Agent J H Cohoon Engineering Limited

Address 440 Hardy Road, Unit 1

Town and Postal Code Brantford, Ontario N3T 5L8

Phone Number 519 753 2656

Cell Number _____

Email rphillips@cohooneng.com

Please specify to whom all communications should be sent. Unless otherwise directed, all correspondence and notices in respect of this application will be forwarded to the agent noted above.

☐ Owner

☒ Agent

☐ Applicant

Names and addresses of any holder of any mortgagees, charges or other encumbrances on the subject lands:

N/A

B. Location, Legal Description and Property Information

1. Legal Description (include Geographic Township, Concession Number, Lot Number, Block Number and Urban Area or Hamlet):

All of Lots 79 & 80, Part of Lots 69, 70 & 78, Part of Railway Lot 2, all in Block 109, Registered Plan 182, Town of Simcoe, Norfolk County

Municipal Civic Address: 205 Metcalfe Street South

Present Official Plan Designation(s): Urban Residential

Present Zoning: CN - Neighbourhood Commercial Zone

2. Is there a special provision or site specific zone on the subject lands?

☐ Yes ☒ No If yes, please specify:

3. Present use of the subject lands:

Vacant

4. Please describe all existing buildings or structures on the subject lands and whether they are to be retained, demolished or removed. If retaining the buildings or structures, please describe the type of buildings or structures, and illustrate the setback, in metric units, from front, rear and side lot lines, ground floor area, gross floor area, lot coverage, number of storeys, width, length, and height on your attached sketch which must be included with your application:

N/A

5. If an addition to an existing building is being proposed, please explain what it will be used for (for example: bedroom, kitchen, or bathroom). If new fixtures are proposed, please describe.

Refer to Drawings attached

6. Please describe **all proposed** buildings or structures/additions on the subject lands. Describe the type of buildings or structures/additions, and illustrate the setback, in metric units, from front, rear and side lot lines, ground floor area, gross floor area, lot coverage, number of storeys, width, length, and height on your attached sketch which must be included with your application:

Refer to Drawings attached

7. Are any existing buildings on the subject lands designated under the *Ontario Heritage Act* as being architecturally and/or historically significant? Yes ☐ No ☒

If yes, identify and provide details of the building:

8. If known, the length of time the existing uses have continued on the subject lands:
Unknown

9. Existing use of abutting properties:
Residential

10. Are there any existing easements or restrictive covenants affecting the subject lands?
☐ Yes ☒ No If yes, describe the easement or restrictive covenant and its effect:

Note: Please complete all that apply.

1. Please explain what you propose to do on the subject lands/premises which makes this development application necessary:

Proposed six (6) semi-detached units on the subject lands.

2. Please explain why it is not possible to comply with the provision(s) of the Zoning By-law/and or Official Plan:

The lot sizes are such that a minor rezoning is required on the property. Refer to attached letter for the details on the modifications to the zoning being requested.

3. Does the requested amendment alter all or any part of the boundary of an area of settlement in the municipality or implement a new area of settlement in the municipality? ☐ Yes ☒ No If yes, describe its effect:

4. Does the requested amendment remove the subject land from an area of employment? ☐ Yes ☒ No If yes, describe its effect:

5. Does the requested amendment alter, replace, or delete a policy of the Official Plan?
☐ Yes ☒ No If yes, identify the policy, and also include a proposed text of the policy amendment (if additional space is required, please attach a separate sheet):

6. Description of land intended to be severed in metric units:

Frontage: 80.17
Depth: Varies 38.57m
Width: 80.17 (Grove Street)
Lot Area: 0.264
Present Use: Vacant
Proposed Use: Residential
Proposed final lot size (if boundary adjustment): N/A

If a boundary adjustment, identify the assessment roll number and property owner of the lands to which the parcel will be added: _____

Description of land intended to be retained in metric units:

Frontage: N/A
Depth: _____
Width: _____
Lot Area: _____
Present Use: _____
Proposed Use: _____
Buildings on retained land: _____

7. Description of proposed right-of-way/easement:

Frontage: N/A
Depth: _____
Width: _____
Area: _____
Proposed use: _____

8. Name of person(s), if known, to whom lands or interest in lands to be transferred, leased or charged (if known):

Unknown

9. Site Information**Existing****Proposed**

Please indicate unit of measurement, for example: m, m² or %

Lot frontage	Refer to Draft Plan	
Lot depth	And attached sketch	
Lot width		
Lot area		
Lot coverage		
Front yard		
Rear yard		
Left Interior side yard		
Right Interior side yard		
Exterior side yard (corner lot)		
Landscaped open space		
Entrance access width		
Exit access width		
Size of fencing or screening		
Type of fencing		

10. Building Size

Number of storeys	TBD	
Building height		
Total ground floor area		
Total gross floor area		
Total useable floor area		

11. Off Street Parking and Loading Facilities

Number of off street parking spaces	
Number of visitor parking spaces	
Number of accessible parking spaces	
Number of off street loading facilities	

12. Residential (if applicable)

Number of buildings existing: 0

Number of buildings proposed: 6 semi-detached (12 units)

Is this a conversion or addition to an existing building? ☐ Yes ☒ No

If yes, describe: _____

Type	Number of Units	Floor Area per Unit in m ²
Single Detached	_____	_____
Semi-Detached	<u>6</u>	<u>104.1 sq.m.</u>
Duplex	_____	_____
Triplex	_____	_____
Four-plex	_____	_____
Street Townhouse	_____	_____
Stacked Townhouse	_____	_____
Apartment - Bachelor	_____	_____
Apartment - One bedroom	_____	_____
Apartment - Two bedroom	_____	_____
Apartment - Three bedroom	_____	_____

Other facilities provided (for example: play facilities, underground parking, games room, or swimming pool): _____

13. Commercial/Industrial Uses (if applicable)

Number of buildings existing: _____

Number of buildings proposed: _____

Is this a conversion or addition to an existing building? ☐ Yes ☐ No

If yes, describe: _____

Indicate the gross floor area by the type of use (for example: office, retail, storage): _____

Seating Capacity (for assembly halls or similar): _____

Total number of fixed seats: _____

Describe the type of business(es) proposed: _____

Total number of staff proposed initially: _____

Total number of staff proposed in five years: _____

Maximum number of staff on the largest shift: _____

Is open storage required: ☐ Yes ☐ No

Is a residential use proposed as part of, or accessory to commercial/industrial use?

☐ Yes ☐ No If yes please describe: _____

14. Institutional (if applicable)

Describe the type of use proposed: _____

Seating capacity (if applicable): _____

Number of beds (if applicable): _____

Total number of staff proposed initially: _____

Total number of staff proposed in five years: _____

Maximum number of staff on the largest shift: _____

Indicate the gross floor area by the type of use (for example: office, retail, or storage): _____

15. Describe Recreational or Other Use(s) (if applicable)

D. Previous Use of the Property

1. Has there been an industrial or commercial use on the subject lands or adjacent lands? ☐ Yes ☐ No ☒ Unknown
If yes, specify the uses (for example: gas station or petroleum storage):
2. Is there reason to believe the subject lands may have been contaminated by former uses on the site or adjacent sites? ☐ Yes ☒ No ☐ Unknown
3. Provide the information you used to determine the answers to the above questions:
Personal Knowledge
4. If you answered yes to any of the above questions in Section D, a previous use inventory showing all known former uses of the subject lands, or if appropriate, the adjacent lands, is needed. Is the previous use inventory attached? ☐ Yes ☒ No

E. Provincial Policy

1. Is the requested amendment consistent with the provincial policy statements issued under subsection 3(1) of the *Planning Act*, R.S.O. 1990, c. P. 13? ☒ Yes ☐ No
If no, please explain:

2. It is owner's responsibility to be aware of and comply with all relevant federal or provincial legislation, municipal by-laws or other agency approvals, including the Endangered Species Act, 2007. Have the subject lands been screened to ensure that development or site alteration will not have any impact on the habitat for endangered or threatened species further to the provincial policy statement subsection 2.1.7? ☐ Yes ☒ No

If no, please explain:

N/A

3. Have the subject lands been screened to ensure that development or site alteration will not have any impact on source water protection? ☒ Yes ☐ No

If no, please explain:

Residential Development

Note: If in an area of source water Wellhead Protection Area (WHPA) A, B or C please attach relevant information and approved mitigation measures from the Risk Manager Official.

4. Are any of the following uses or features on the subject lands or within 500 metres of the subject lands, unless otherwise specified? Please check boxes, if applicable.

Livestock facility or stockyard (submit MDS Calculation with application)

☐ On the subject lands or ☐ within 500 meters – distance _____

Wooded area

☐ On the subject lands or ☐ within 500 meters – distance _____

Municipal Landfill

☐ On the subject lands or ☐ within 500 meters – distance _____

Sewage treatment plant or waste stabilization plant

☐ On the subject lands or ☐ within 500 meters – distance _____

Provincially significant wetland (class 1, 2 or 3) or other environmental feature

☐ On the subject lands or ☐ within 500 meters – distance _____

Floodplain

☐ On the subject lands or ☐ within 500 meters – distance _____

Rehabilitated mine site

☐ On the subject lands or ☐ within 500 meters – distance _____

Non-operating mine site within one kilometre

☐ On the subject lands or ☐ within 500 meters – distance _____

Active mine site within one kilometre

☐ On the subject lands or ☐ within 500 meters – distance _____

Industrial or commercial use (specify the use(s))

☐ On the subject lands or ☐ within 500 meters – distance _____

Active railway line

☐ On the subject lands or ☐ within 500 meters – distance _____

Seasonal wetness of lands

☐ On the subject lands or ☐ within 500 meters – distance _____

Erosion

☐ On the subject lands or ☐ within 500 meters – distance _____

Abandoned gas wells

☐ On the subject lands or ☐ within 500 meters – distance _____

F. Servicing and Access

1. Indicate what services are available or proposed:

Water Supply

- ☒ Municipal piped water
☐ Individual wells

- ☐ Communal wells
☐ Other (describe below)
-

Sewage Treatment

- ☒ Municipal sewers
☐ Septic tank and tile bed in good working order
- ☐ Communal system
☐ Other (describe below)
-

Storm Drainage

- ☒ Storm sewers
☐ Other (describe below)
- ☐ Open ditches
-

2. Existing or proposed access to subject lands:

- ☒ Municipal road
☐ Unopened road
- ☐ Provincial highway
☐ Other (describe below)

Name of road/street: Grove Street

G. Other Information

1. Does the application involve a local business? ☐ Yes ☒ No
If yes, how many people are employed on the subject lands?
-

2. Is there any other information that you think may be useful in the review of this application? If so, explain below or attach on a separate page.
-

H. Supporting Material to be submitted by Applicant

In order for your application to be considered complete, **folded** hard copies (number of paper copies as directed by the planner) and an **electronic version (PDF) of the properly named site plan drawings, additional plans, studies and reports** will be required, including but not limited to the following details:

1. Concept/Layout Plan
2. All measurements in metric
3. Key map
4. Scale, legend and north arrow
5. Legal description and municipal address
6. Development name
7. Drawing title, number, original date and revision dates
8. Owner's name, address and telephone number
9. Engineer's name, address and telephone number
10. Professional engineer's stamp
11. Existing and proposed easements and right of ways
12. Zoning compliance table – required versus proposed
13. Parking space totals – required and proposed
14. All entrances to parking areas marked with directional arrows
15. Loading spaces, facilities and routes (for commercial developments)
16. All dimensions of the subject lands
17. Dimensions and setbacks of all buildings and structures
18. Location and setbacks of septic system and well from all existing and proposed lot lines, and all existing and proposed structures
19. Gross, ground and useable floor area
20. Lot coverage
21. Floor area ratio
22. Building entrances, building type, height, grades and extent of overhangs
23. Names, dimensions and location of adjacent streets including daylighting triangles
24. Driveways, curbs, drop curbs, pavement markings, widths, radii and traffic directional signs
25. All exterior stairways and ramps with dimensions and setbacks
26. Retaining walls including materials proposed
27. Fire access and routes
28. Location, dimensions and number of parking spaces (including visitor and accessible) and drive aisles
29. Location of mechanical room, and other building services (e.g. A/C, HRV)
30. Refuse disposal and storage areas including any related screening (if indoors, need notation on site plan)
31. Winter snow storage location

32. Landscape areas with dimensions
33. Natural features, watercourses and trees
34. Fire hydrants and utilities location
35. Fencing, screening and buffering – size, type and location
36. All hard surface materials
37. Light standards and wall mounted lights (plus a note on the site plan that all outdoor lighting is to be dark sky compliant)
38. Business signs (make sure they are not in sight lines)
39. Sidewalks and walkways with dimensions
40. Pedestrian access routes into site and around site
41. Bicycle parking
42. Architectural elevations of all building sides
43. All other requirements as per the pre-consultation meeting

In addition, the following additional plans, studies and reports, including but not limited to, **may** also be required as part of the complete application submission:

- ☐ Zoning Deficiency Form
- ☐ On-Site Sewage Disposal System Evaluation Form (to verify location and condition)
- ☐ Architectural Plan
- ☐ Buildings Elevation Plan
- ☐ Cut and Fill Plan
- ☐ Erosion and Sediment Control Plan
- ☐ Grading and Drainage Control Plan (around perimeter and within site) (existing and proposed)
- ☐ Landscape Plan
- ☐ Photometric (Lighting) Plan
- ☐ Plan and Profile Drawings
- ☐ Site Servicing Plan
- ☐ Storm water Management Plan
- ☐ Street Sign and Traffic Plan
- ☐ Street Tree Planting Plan
- ☐ Tree Preservation Plan
- ☐ Archaeological Assessment
- ☐ Environmental Impact Study

- ☐ Geotechnical Study / Hydrogeological Review
- ☐ Minimum Distance Separation Schedule
- ☐ Noise or Vibration Study
- ☐ Record of Site Condition
- ☐ Storm water Management Report
- ☐ Traffic Impact Study – please contact the Planner to verify the scope required

Site Plan applications will require the following supporting materials:

1. Two (2) complete sets of the site plan drawings folded to 8½ x 11 and an electronic version in PDF format
2. Letter requesting that the Holding be removed (if applicable)
3. A cost estimate prepared by the applicant's engineer
4. An estimate for Parkland dedication by a certified land appraiser
5. Property Identification Number (PIN) printout

Standard condominium exemptions will require the following supporting materials:

- ☐ Plan of standard condominium (2 paper copies and 1 electronic copy)
- ☐ Draft condominium declaration
- ☐ Property Identification Number (PIN) printout

Your development approval might also be dependent on Ministry of Environment and Climate Change, Ministry of Transportation or other relevant federal or provincial legislation, municipal by-laws or other agency approvals.

All final plans must include the owner's signature as well as the engineer's signature and seal.

I. Development Agreements

A development agreement may be required prior to approval for site plan, subdivision and condominium applications. Should this be necessary for your development, you will be contacted by the agreement administrator with further details of the requirements including but not limited to insurance coverage, professional liability for your engineer, additional fees and securities.

J. Transfers, Easements and Postponement of Interest

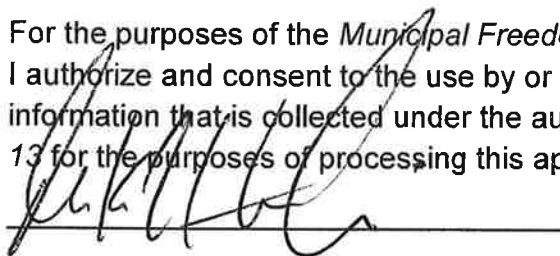
The owner acknowledges and agrees that if required it is their solicitor's responsibility on behalf of the owner for the registration of all transfer(s) of land to the County, and/or transfer(s) of easement in favour of the County and/or utilities. Also, the owner further acknowledges and agrees that it is their solicitor's responsibility on behalf of the owner for the registration of postponements of any charges in favour of the County.

K. Permission to Enter Subject Lands

Permission is hereby granted to Norfolk County officers, employees or agents, to enter the premises subject to this application for the purposes of making inspections associated with this application, during normal and reasonable working hours.

L. Freedom of Information

For the purposes of the *Municipal Freedom of Information and Protection of Privacy Act*, I authorize and consent to the use by or the disclosure to any person or public body any information that is collected under the authority of the *Planning Act*, R.S.O. 1990, c. P. 13 for the purposes of processing this application.



Owner/Applicant Signature

February 25, 2021

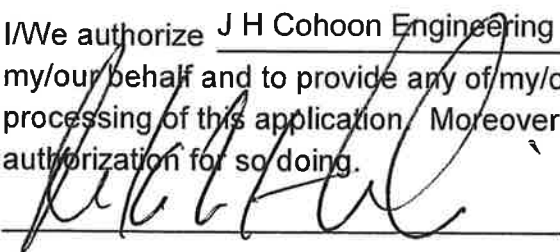
Date

M. Owner's Authorization

If the applicant/agent is not the registered owner of the lands that is the subject of this application, the owner(s) must complete the authorization set out below.

I/We Mike Quattrociocchi am/are the registered owner(s) of the lands that is the subject of this application.

I/We authorize J H Cohoon Engineering Limited to make this application on my/our behalf and to provide any of my/our personal information necessary for the processing of this application. Moreover, this shall be your good and sufficient authorization for so doing.



Owner

February 25, 2021

Date

Owner

Date


N. Declaration

I, R W Phillips of Brantford

solemnly declare that:

all of the above statements and the statements contained in all of the exhibits transmitted herewith are true and I make this solemn declaration conscientiously believing it to be true and knowing that it is of the same force and effect as if made under oath and by virtue of *The Canada Evidence Act*.

Declared before me at:


Owner/Applicant Signature

In BRANTFORD / COUNTY OF BRANT

This 26th day of FEB

A.D., 20____



A Commissioner, etc.

LINDA ELAINE CLARKSON,
a Commissioner, etc., Province of Ontario
for J.H. Cohoon Engineering Limited.
Expires June 5, 2021



14316

J.H. COHOON ENGINEERING LIMITED

CONSULTING ENGINEERS

440 Hardy Road, Unit #1, Brantford, ON N3T 5L8
Tel: (519) 753-2656 Fax: (519) 753-4263
www.cohooneng.com

February 25, 2021

County of Norfolk
Planning Development and Cultural Services Division
22 Albert Street
Langton, Ontario
N0E 1G0

Attention: Ms. T. Givens, MCIP, RPP
Interim Director of Planning

Re: Re-zoning Application and Draft Plan of Subdivision
Grove Street (at Metcalfe)
Simcoe, Ontario
Norfolk County
Re-zoning Application and Draft Plan of Subdivision

Dear Ms. Givens:

Further to our discussions, and on behalf of our client, Mr. Mike Quattrociocchi of Mayberry Homes, please find enclosed the following information regarding our application for a redline revision to the current draft approval and re-zoning on the subject lands for a residential plan of subdivision located in Simcoe, Ontario at the corner of Grove and Metcalfe Street.

1. Five (5) copies of the "Draft Plan of Subdivision", illustrating the proposed lot confirmation.
2. Three (3) copies of the Application for Draft Approval and Re-Zoning of the lands to allow the proposed development.
3. A cheque in the amount of \$ 11,237.00 (relating to the required \$ 3,802.00 relating to the rezoning of the lands and \$ 7,435.00 for the draft plan of subdivision).
4. Five (5) copies of the Lot Statistics Sketch prepared by our firm to illustrate the deficiencies in the zoning.
5. One (1) copy of the traffic impact statement regarding the proposed development dated Feb 25, 2021 as prepared by our firm
6. One (1) copy of the functional servicing report prepared by our office relating to the above noted development.
7. One (1) copy of current zoning provisions for this site.



Professional Engineers
Ontario

8. Electronic copies of this documentation will be provided to your office.

The proposal is to create a residential plan of subdivision that consists of six (6) semidetached units. The following items were identified as requiring some modification for this proposed development to the CN zone (which refers back to the R2 Zone category).

Side yard Reduction

0.76 m whereas the bylaw requires 1.2m (interior Lot line setback)

Lot Area Reduction

Interior individual units to 187 sq.m. whereas the bylaw requires a minimum of 255 sq.m. per unit.

Interior Lot Area Reduction for Original Lots to 388 sq.m. whereas the bylaw requires 510 sq.m.

Exterior Lot Area Reduction for Original Lots to 590 sq.m. whereas the bylaw requires 690 sq.m.

Lot Frontage

Lot frontage for Original Lots to 10.06 m whereas the bylaw requires 17.0m per semidetached unit (8.5m per unit)

Lot frontage for Industrial Units to 5.03m whereas the bylaw requires 8.5m per unit

Lot frontage for the original lot to 15.39m whereas the bylaw requires a total of 20.0m

Lot frontage for the corner lot to 10.36m whereas the bylaw requires a frontage of 11.5m

The proposal is to create the semi-detached lots by a plan of subdivision with the individual units being created by Part Lot control in the future.

With the submission of this information, we would respectfully your prompt circulation of the application to the various departments.

If you require any further details please do not hesitate to contact this office, at your earliest convenience.

Yours truly,

J.H. COHOON ENGINEERING LIMITED

R.W. Phillips, P.Eng.

c.c. Mike Quattrociocchi – Mayberry Homes

DRAFT PLAN OF SUBDIVISION

ALL OF LOTS 79 & 80
PART OF LOTS 69, 70 & 78
PART OF RAILWAY LOT 2
ALL IN BLOCK 109
REGISTERED PLAN 182
TOWN OF SIMCOE
NORFOLK COUNTY

KEY PLAN



SURVEYOR'S CERTIFICATE

I HEREBY CERTIFY THAT THE BOUNDARIES OF THE LANDS TO BE SUBDIVIDED AS SHOWN ON THIS PLAN AND THEIR RELATIONSHIP TO THE ADJACENT LANDS ARE ACCURATELY AND CORRECTLY SHOWN.

Feb 16, 2021
DATE

JIM JOHNSON, O.L.S.
WEST & RUUSKA LTD.

OWNER'S CERTIFICATE

I HEREBY AUTHORIZE J.H. COHOON ENGINEERING LTD. TO PREPARE AND SUBMIT THIS DRAFT PLAN OF SUBDIVISION TO THE CORPORATION OF NORFOLK COUNTY FOR APPROVAL.

Feb 25, 2021
DATE

MAYBERRY HOMES

ADDITIONAL INFORMATION REQUIRED UNDER SECTION 51(17) OF THE PLANNING ACT

A - SEE PLAN	G - SEE PLAN
B - SEE PLAN	H - MUNICIPAL WATER
C - SEE PLAN	I - SAND AND GRAVEL
D - RESIDENTIAL (SEMI-DETACHED)	J - SEE PLAN
E - SEE PLAN	K - SANITARY & STORM SEWERS
F - SEE PLAN	L - NONE

AREA SCHEDULE

TOTAL AREA = 0.264 ha

J.H. COHOON
ENGINEERING
LIMITED
CONSULTING ENGINEERS

440 HARDY ROAD, UNIT #1, BRANTFORD - ONTARIO, N3T 5J4
TEL: (519) 753-2656 FAX: (519) 753-4283 www.cohooneg.com

DESIGN: R.W.P.

DRAWN: K.P.B.

SCALE: 1:200

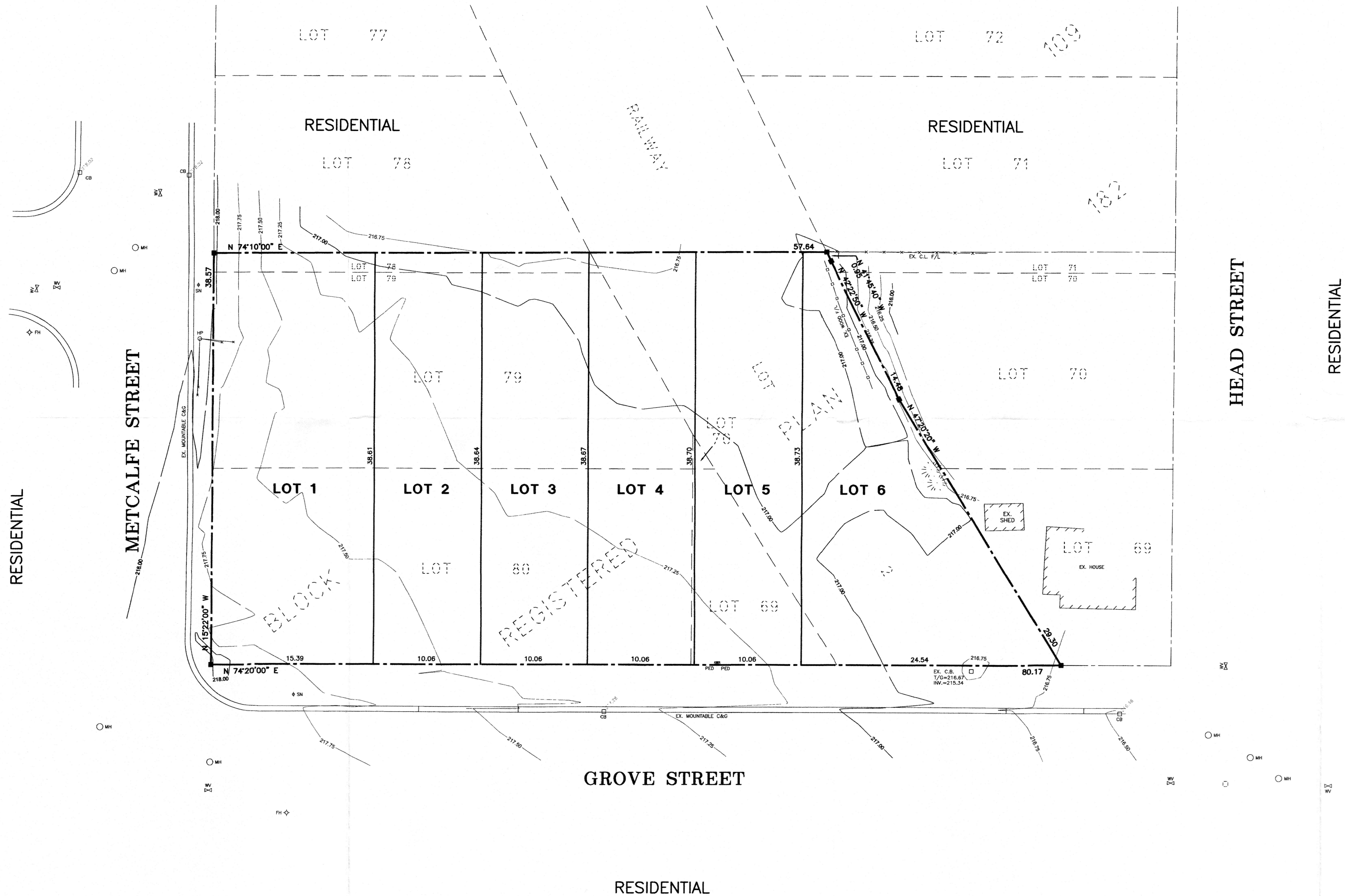
DATE: FEB. 9/21

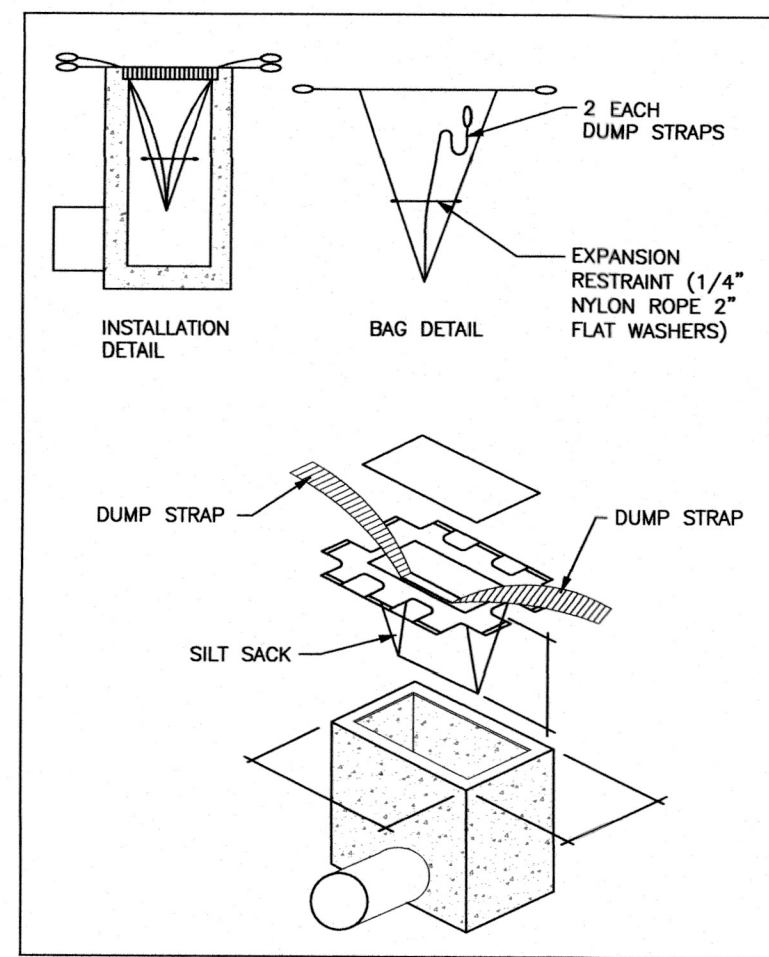
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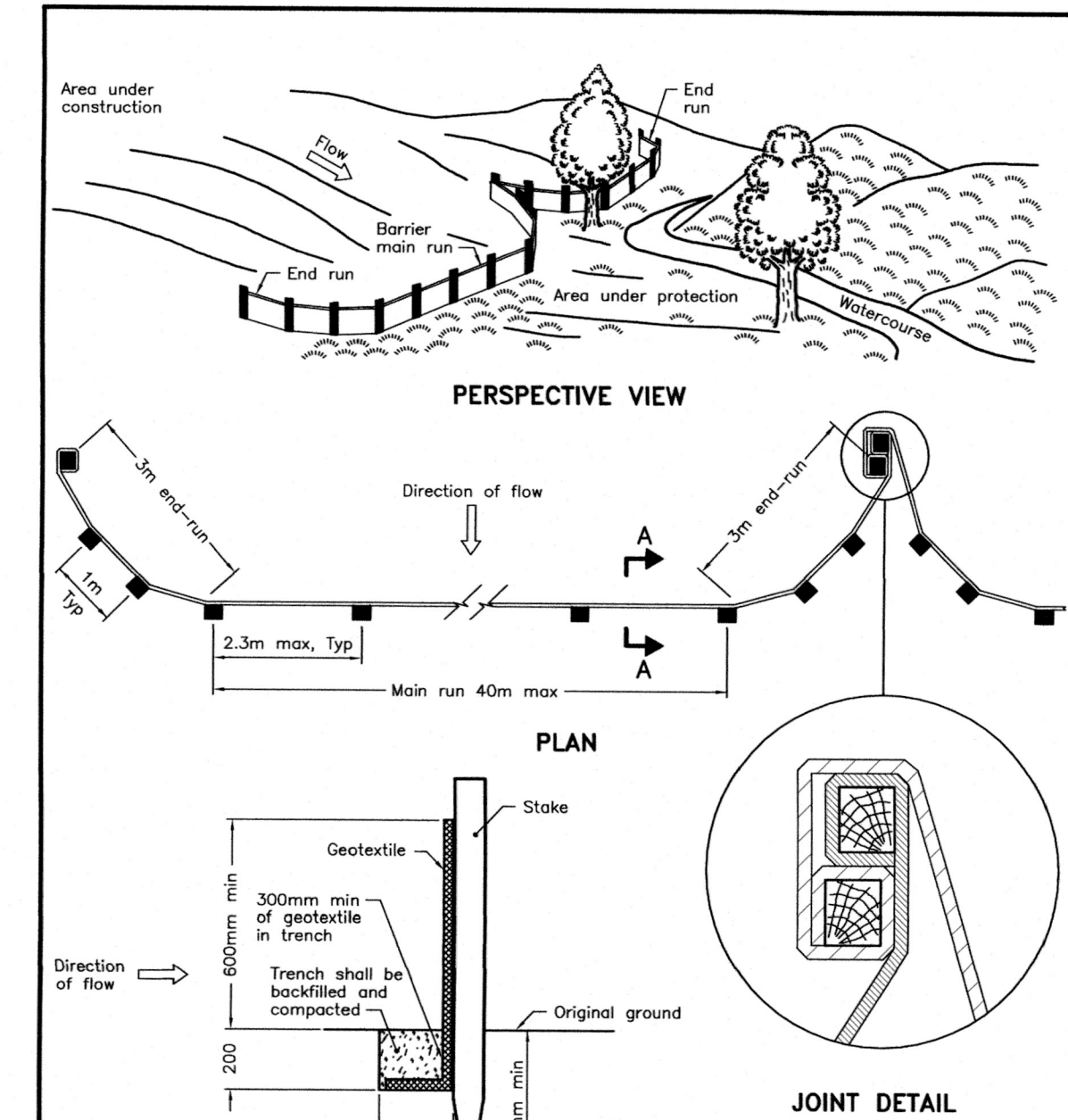
DWG. No:

DP1





SILT SACK DETAIL
N.T.S.



NOTE:
A All dimensions are in millimetres unless otherwise shown.

ONTARIO PROVINCIAL STANDARD DRAWING	Nov 2015	Rev 2
LIGHT-DUTY SILT FENCE BARRIER		
		OPSD 219.110

THE POSITION OF POLE LINES, CONDUITS, WATERMANS, SEWERS AND OTHER UNDERGROUND AND ABOVEGROUND UTILITIES AND STRUCTURES ARE NOT NECESSARILY SHOWN ON THE CONTRACT DRAWINGS, AND WHERE SHOWN, THE ACCURACY OF THE POSITION OF SUCH UTILITIES AND STRUCTURES IS NOT GUARANTEED. BEFORE COMMENCING WORK, THE CONTRACTOR SHALL FAMILIARIZE HIMSELF OF THE EXACT LOCATION OF ALL SUCH UTILITIES AND STRUCTURES AND SHALL ASSUME ALL LIABILITY FOR DAMAGE TO THEM.

- LEGEND:**
- EXISTING ELEVATIONS
 - PROPOSED ELEVATIONS
 - PROPOSED ELEVATIONS (ABUTTING DESIGN)
 - PROPOSED SWALE ELEVATIONS
 - PROPOSED SWALE ELEVATIONS (DESIGN)
 - PROPOSED SWALE
 - GENERAL DRAINAGE
 - PROPOSED SILT FENCE
 - SILT SACK AS SHOWN

- NOTES:**
- ALL ELEVATIONS SHOWN ARE METRIC.
 - BUILDER/OWNER TO VERIFY COMPLIANCE WITH ZONING BYLAWS (i.e. SIDEYARDS, SETBACKS, REARYARDS ETC.)
 - WHERE ONLY ONE ELEVATION IS SHOWN, EXISTING AND PROPOSED ELEVATIONS ARE THE SAME.
 - THE SILTATION & EROSION CONTROL (SEC) MEASURES ILLUSTRATED ON THIS PLAN ARE CONSIDERED TO BE THE MINIMUM REQUIREMENT. SITE CONDITIONS MAY REQUIRE ADDITIONAL MEASURES WHICH WILL BE IDENTIFIED BY THE ENGINEER DURING CONSTRUCTION.
 - ALL SEC MEASURES ARE TO BE IN PLACE PRIOR TO COMMENCEMENT OF CONSTRUCTION.
 - OWNER/CONTRACTOR TO MAINTAIN EROSION CONTROL MEASURES THROUGHOUT SITE UNTIL A COMPLETE GRASS/VEGETATION COVER IS ACHIEVED.
 - ONLY AT THE DIRECTION OF THE ENGINEER ARE THE SEC MEASURES TO BE REMOVED.
 - ALL EXPOSED AREAS NOT SUBJECT TO ACTIVE CONSTRUCTION WITHIN 30 DAYS ARE TO BE REVEGETATED AS PER O.P.S.S. 572 IMMEDIATELY UPON COMPLETION OF AREA GRADING.
 - CONTRACTOR TO PROVIDE SILT FENCE AROUND PERIMETER OF ALL ON SITE STOCKPILES.
 - CONTRACTOR TO PROVIDE SILT SACKS ON TOP OF ALL EXISTING AND PROPOSED STORM STRUCTURES WITHIN THE INFLUENCE OF RUNOFF DURING CONSTRUCTION UNTIL ADEQUATE VEGETATIVE COVER IS ACHIEVED.

T.B.M. No. 1 ELEV. = 218.94m (GEO)
TOP NUT OF FIRE HYDRANT AT THE INTERSECTION OF METCALFE AND GROVE STREETS AS SHOWN.

T.B.M. No. 2 ELEV. = 216.82m (GEO)
TOP NUT OF FIRE HYDRANT AT THE INTERSECTION OF HEAD AND GROVE STREETS AS SHOWN.

NO.	REVISION	DATE (MM/DD/YY)	BY
-----	----------	-----------------	----

**J.H. COHOON
ENGINEERING
LIMITED**
CONSULTING ENGINEERS

440 HARDY ROAD, UNIT #1, BRANTFORD - ONTARIO, N3T 5L8
TEL. (519) 753-2856 FAX. (519) 753-4263 www.cohooneeng.com

PROJECT:
**PROPOSED
RESIDENTIAL
DEVELOPMENT**
GROVE STREET, SIMCOE
NORFOLK COUNTY

CLIENT:
MAYBERRY HOMES

**GRADING & SILTATION AND
EROSION CONTROL PLAN**

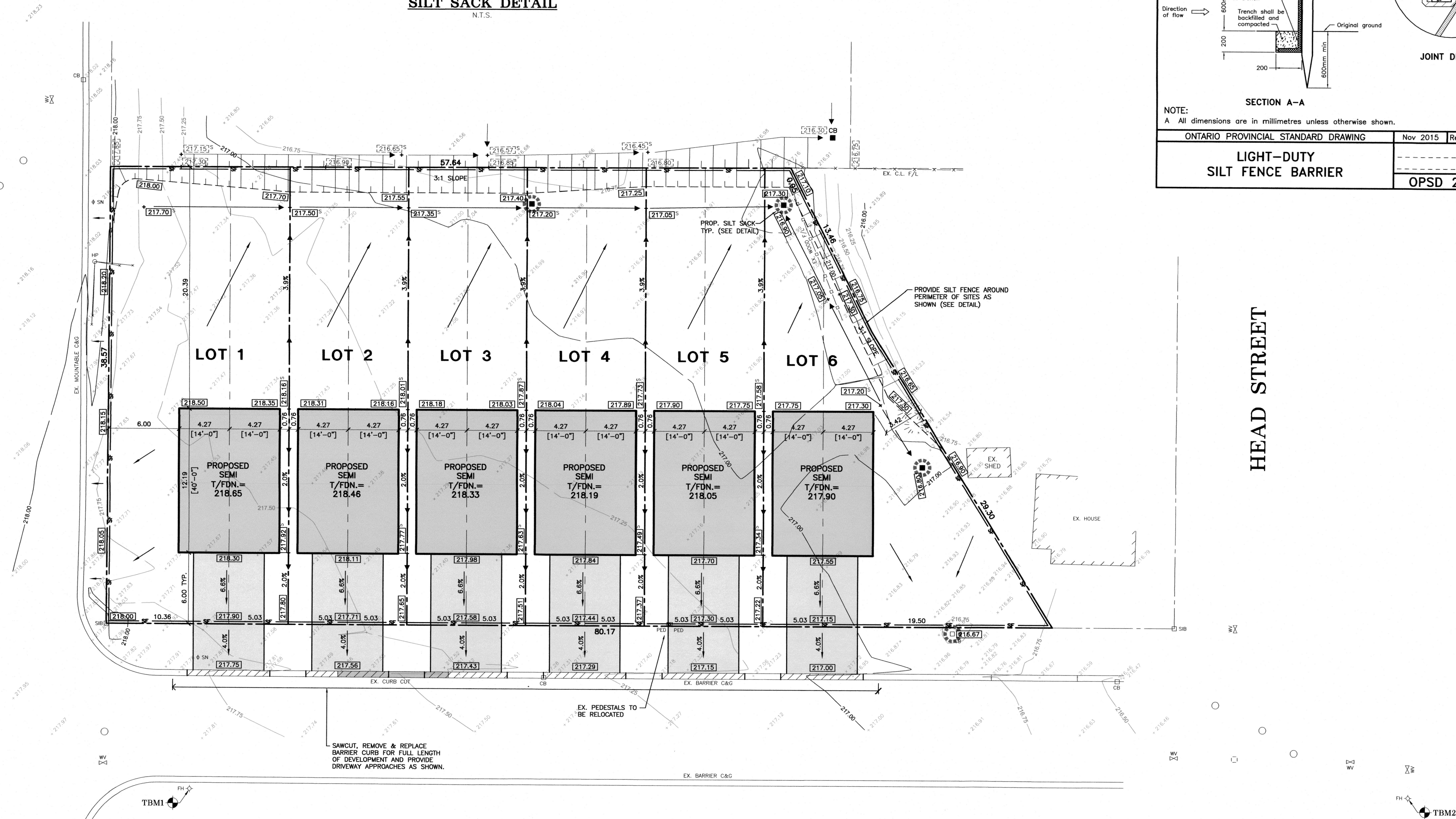
DESIGN: R.W.P.	SCALE: 1:200
DRAWN: K.P.B.	JOB No: 14316
CHECKED: R.W.P.	
SHEET: 1 of 2	DWG. No: 14316-1
DATE: FEB. 26/21	



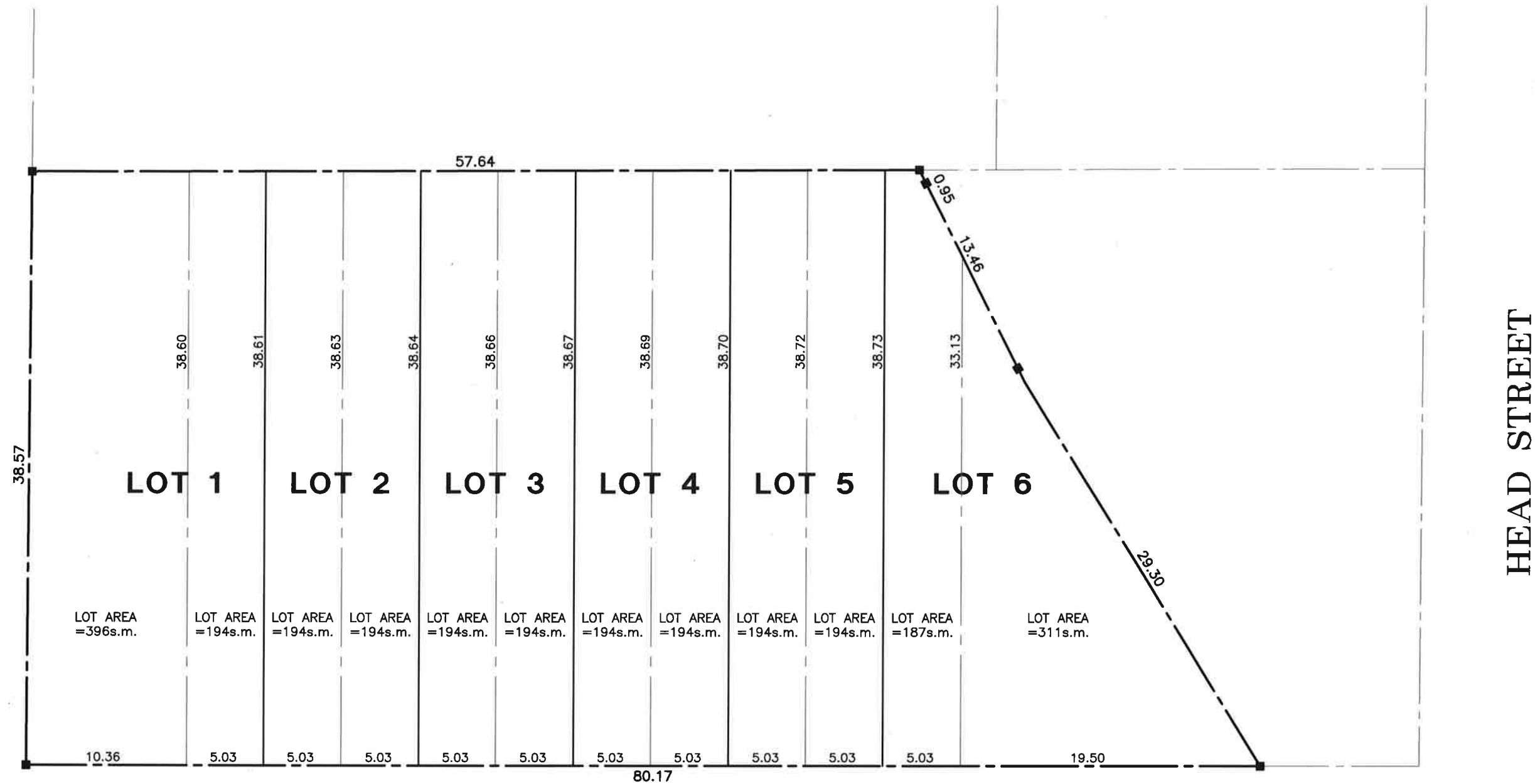
METCALFE STREET

HEAD STREET

GROVE STREET



METCALFE STREET



SEMI DETACHED-R2 ZONE
FRONT=6.0m
REAR=7.5m
INTERIOR SIDE=1.2m
EXTERIOR SIDE=6.0m
LOT AREA INTERIOR=255s.m.
LOT AREA CORNER=345s.m.
LOT FRONTAGE INTERIOR=8.5m.
LOT FRONTAGE CORNER=11.5m.

GROVE STREET

LOT STATISTICS

PROPOSED RESIDENTIAL DEVELOPMENT
GROVE STREET - NORFOLK COUNTY

 **J.H. COHOON ENGINEERING LIMITED**
CONSULTING ENGINEERS
BRANTFORD

CLIENT: MAYBERRY HOMES
SCALE: 1:300

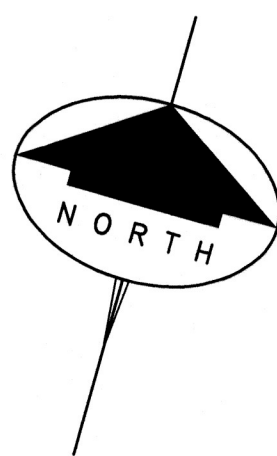
JOB: 14316

SANITARY SEWERS:

1. SANITARY & STORM SEWERS & RELATED APPURTENANCES SHALL BE DESIGNED AND CONSTRUCTED SO AS TO COMPLY WITH APPLICABLE LAW, TO BE CONSISTENT WITH THE COUNTY OF NORFOLK DEVELOPMENT AND ENGINEERING STANDARDS AND IN ACCORDANCE WITH CURRENT GUIDELINES, CODES, REGULATIONS, BEST PRACTICES AND STANDARDS PRESCRIBED BY THE COUNTY.
2. COVER AND BEDDING MATERIAL FOR PVC PIPE AS PER OPSD 802.010 TYPE 2 TRENCH BEDDING SHALL BE GRANULAR 'A' MATERIAL, UNLESS OTHERWISE INDICATED.
3. PVC PIPE WILL REQUIRE SPECIAL CONSTRUCTION PROCEDURES FOR LEAKAGE AND TESTING, PIPE DEFLECTION, ETC.
4. ALTERNATE MATERIALS MAY BE ACCEPTABLE, PROVIDED APPROVAL HAS FIRST BEEN OBTAINED FROM THE COUNTY OF NORFOLK AND ENGINEER IN WRITING.
5. ALL SEWER AND CULVERT INSTALLATIONS TO CONFORM WITH OPSD 802.031 TYPE 3 SOIL.
6. PRIVATE SANITARY DRAINS TO 125mm \varnothing PVC DR28 PIPE AND HAVE A MIN. GRADE OF 2.0%, AS PER NORFOLK COUNTY DESIGN CRITERIA.
7. A 38x89mm x 2.0m LONG MARKER IS TO BE PLACED FROM THE CAPPED LATERAL AND EXTEND 1.0m ABOVE GROUND AND PAINTED GREEN FOR SANITARY AND WHITE FOR STORM.
8. BEDDING FOR PRIVATE SANITARY & STORM DRAINS AS PER OPSD 1006.02 TYPE 2 TRENCH WITH GRANULAR 'A' BEDDING AND COVER MATERIAL.
9. MINIMUM FALL FOR PRIVATE SANITARY & STORM DRAINS TO BE 2.0%

GENERAL NOTES:

1. CONSTRUCTION OF SEWERS, WATERMAINS AND RELATED APPURTENANCES SHALL BE UNDERTAKEN IN ACCORDANCE WITH THE CURRENT STANDARD DRAWINGS OF THE COUNTY OF NORFOLK, AND THE ONTARIO PROVINCIAL STANDARD DRAWINGS (OPSD). THE COUNTY OF NORFOLK DRAWINGS SHALL TAKE PRECEDENCE OVER THE OPSD DRAWINGS.
2. INFORMATION REGARDING ANY EXISTING SERVICES AND/OR UTILITIES SHOWN ON THE APPROVED SET OF CONSTRUCTION DRAWINGS IS FURNISHED AS THE BEST AVAILABLE INFORMATION. THE CONTRACTOR SHALL INTERPRET THIS INFORMATION AS THEY SEE FIT WITH THE UNDERSTANDING THAT THE OWNER AND HIS AGENTS DISCLAIM ALL RESPONSIBILITY FOR ITS ACCURACY AND/OR SUFFICIENCY.
3. ALL DIMENSIONS SHALL BE CHECKED AND VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO ANY CONSTRUCTION AND HE SHALL REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ENGINEER.
4. RELOCATION OF EXISTING SERVICES AND/OR UTILITIES SHALL BE CONSTRUCTED AS SHOWN ON THE DRAWINGS OR AS DIRECTED BY THE ENGINEER.
5. THE CONTRACTOR SHALL OBTAIN ALL PERMITS FOR CONSTRUCTION.
6. FOR ALL SEWERS AND WATERMAIN IN FILL SECTIONS, THE COMPACTION SHALL BE VERIFIED PRIOR TO LAYING OF PIPE.
7. NO SUBSTITUTIONS WILL BE ALLOWED WITHOUT WRITTEN APPROVAL FROM THE COUNTY OF NORFOLK OR THE ENGINEER.
8. NO BLASTING WILL BE PERMITTED.
9. ALL EXCAVATIONS TO BE BACKFILLED WITH SELECT NATIVE MATERIAL, APPROVED BY THE ENGINEER, TO 95% S.P.D.
10. THE DEVELOPER AND/OR CONTRACTOR IS RESPONSIBLE FOR INSTALLING AND MAINTAINING (UNTIL ROAD CONSTRUCTION IS FINISHED) SILT CONTROL DEVICES AS SHOWN ON THE DRAWINGS AND AS DIRECTED BY THE ENGINEER.
11. ALL WORKS SHALL BE DESIGNED AND CONSTRUCTED SO AS TO COMPLY WITH APPLICABLE LAW, TO BE CONSISTENT WITH THE COUNTY OF NORFOLK DEVELOPMENT & ENGINEERING STANDARDS AND IN ACCORDANCE WITH CURRENT GUIDELINES, CODES, REGULATIONS AND STANDARDS PRESCRIBED BY THE COUNTY.
12. ALL BOULEVARD AREAS TO BE RESTORED WITH #1 NURSERY SOD ON A MINIMUM 100mm OF SELECT TOPSOIL.
13. ALL TRENCH BACKFILL UNDER EXISTING ROADWAYS SHALL BE COMPACTED IN MINIMUM 230mm LIFTS TO 98% STANDARD PROCTOR DENSITY. A GEOTECHNICAL ENGINEER'S REPRESENTATIVE SHALL BE ON SITE DURING THE WORK TO VERIFY THE COMPACTION OF EACH LIFT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS OF RE-TESTING.
14. AN ENGINEER IS REQUIRED TO BE ONSITE FOR INSPECTION OF ALL UNDERGROUND SERVICES.
15. DRIVEWAYS MUST HAVE A MINIMUM 1.0m CLEARANCE FROM ALL UTILITIES SUCH AS FIRE HYDRANT, STREETLIGHT POLES, TRANSFORMERS, CANADA POST COMMUNITY MAILBOX LOCATIONS, ETC.
16. ALL WATER SERVICE CONNECTIONS 19mm DIA. ASTM B88 TYPE 'K' SOFT COPPER AS PER OPSD 1104.01 & COUNTY OF NORFOLK ENGINEERING STANDARDS, & INSTALLED IN ACCORDANCE WITH OPSD 802.110 TYPE 2 TRENCH BEDDING TO BE GRANULAR 'A'.
17. CURB STOPS TO BE MUELLER A-726 OR EQUIVALENT APPROVED BY THE COUNTY OF NORFOLK.
18. CATHODIC PROTECTION TO BE PROVIDED AT ALL VALVES, BENDS AND FITTINGS WITH 11.0 KG ZINC ANODES AND ON ALL WATER SERVICE CONNECTIONS WITH 5.5 KG ZINC ANODES.



LEGEND:

	EXIST. STORM SEWER SYSTEM
	EXIST. SANITARY SEWER SYSTEM
	EXISTING WATERMAIN
	EXISTING CHAIN LINK FENCE
	EXISTING WOOD FENCE
	EXISTING SANITARY MANHOLE
	EXISTING STORM MANHOLE
	EXISTING CATCHBASIN
	EXISTING BELL PEDESTAL
	EXIST. WATER VALVE
	EXIST. FIRE HYDRANT
	EXISTING HYDRO POLE
	EXISTING TEMPORARY BENCHMARK
	EXISTING SIGN
	PROPOSED SANITARY SERVICE
	PROPOSED WATER SERVICE

T.B.M. No. 1 ELEV. = 218.94m (GEO)

TOP NUT OF FIRE HYDRANT AT THE INTERSECTION OF METCALFE AND GROVE STREETS AS SHOWN.

T.B.M. No. 2 ELEV. = 216.82m (GEO)

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NO. REVISION DATE (MM/DD/YY) BY



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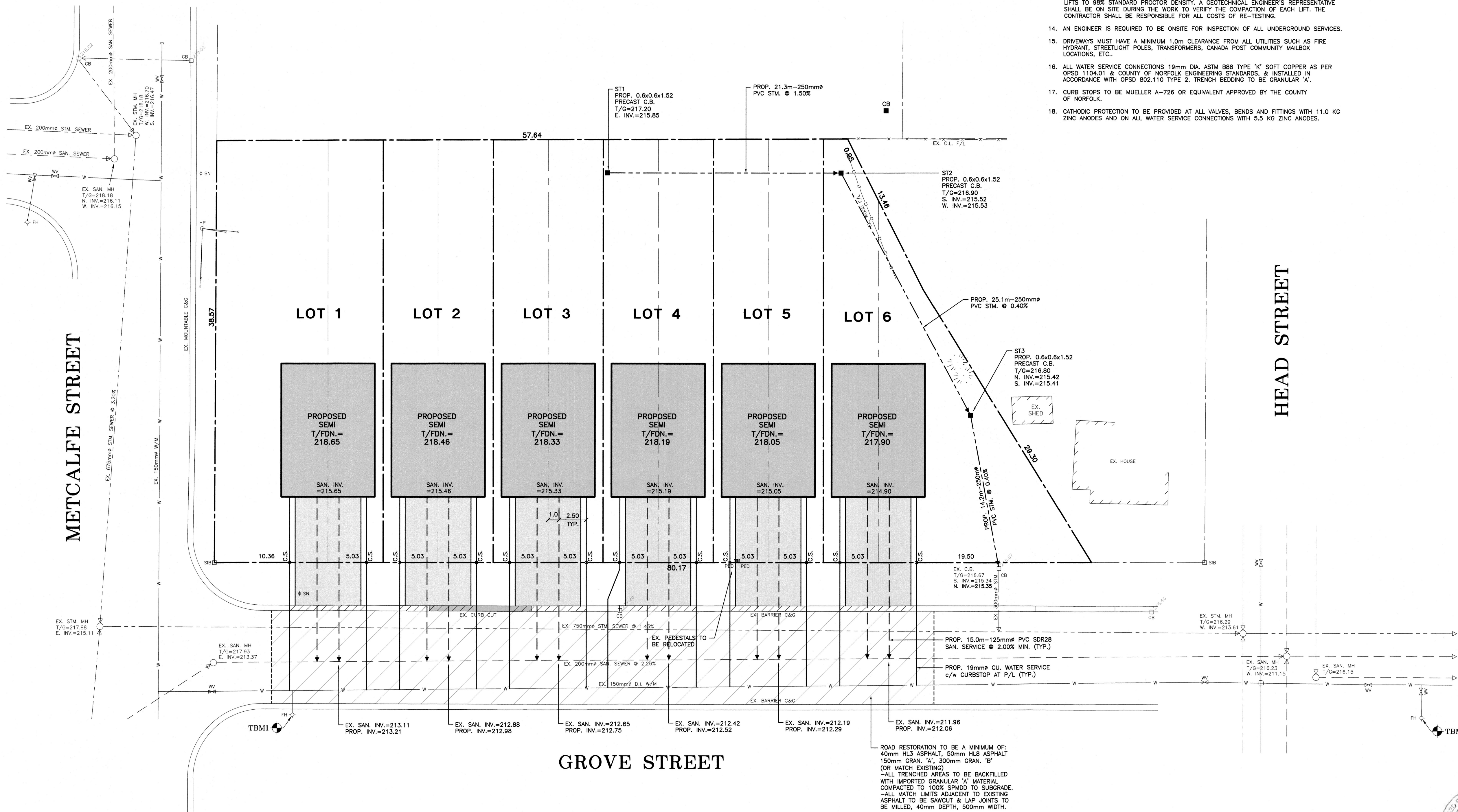
PROJECT:

PROPOSED RESIDENTIAL DEVELOPMENT
GROVE STREET, SIMCOE
NORFOLK COUNTY

CLIENT: MAYBERRY HOMES

SERVICING PLAN

DESIGN: R.W.P.	SCALE: 1:200
DRAWN: K.P.B.	JOB No: 14316
CHECKED: R.W.P.	
SHEET: 2 of 2	DWG. No: 14316-2
DATE: FEB. 26/21	





J.H. COHOON ENGINEERING LIMITED

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440 Hardy Road, Unit #1, Brantford, ON N3T 5L8
Tel: (519) 753-2656 Fax: (519) 753-4263
www.cohooneng.com

14316

February 25, 2021

Norfolk County
Norfolk County Planning Department
County of Norfolk
Planning Development and Cultural Services Division
185 Robinson Street, Suite 200
Simcoe Ontario
N3Y 5L6

Attention: Ms. T. Givens, MCIP, RPP
Interim Director of Planning

Re: Proposed Residential Draft Plan of Subdivision
Simcoe, Ontario
Grove Street Development
Norfolk County
Traffic Considerations

Dear Ms. Givens:

In response to a request of the Norfolk County during the pre-consultation meeting and further to our previous submission dated December 16, 2020, we have prepared this correspondence to address the traffic considerations of the impacts of this proposed development on the surrounding traffic network in the area.

To assist, please find attached the following information relating to this development:

- Traffic Counts as provided by the Norfolk County Public Works Department
 - Chapel Street between Talbot and Colborne Street
 - 2006 1238 AADT
 - 2009 1067 AADT
 - Stanley Street between Talbot and Colborne Street
 - 2006 2350 AADT
 - 2009 2266 AADT
 - 2012 2228 AADT
 - 2015 2221 AADT
- Google Mapping indicating the site location and the general nature of the area
- An aerial photograph of the area indicating the residential nature of the site area.



Professional Engineers
Ontario

The project involves the development of a vacant site to construct six (6) street facing semi-detached units fronting onto Grove Street in Simcoe, Ontario in Norfolk County.

In reviewing the traffic volumes in the area, the records indicate rather low traffic volumes in the area although the traffic counts are not specifically located adjacent to the site. However, the traffic volumes represent relatively low volumes of traffic in the area. This area is predominately a residential area and with the low traffic volumes and therefore, it is our opinion, that no impacts on the surrounding road network will be experienced.

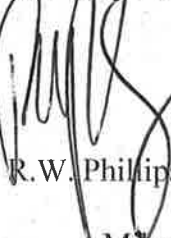
The proposal is to have 12 new driveways on Grove Street. The sight distances on Grove Street are sufficient for the proposed driveway entrances which is similar to the driveways on the west side of the street. The implied speed limit is 50 km/hr. and the associated site distance of 120m is maintained in each direction. Restrictions to the site distances would be parked cars on the street and not vegetation and / or center line profile. The driveways as proposed will not impact the access to the driveways on the south side and will function properly in this location. Grove Street is currently traffic controlled with stop signs at Head Street and Metcalfe Street. The inclusion of the combined driveways also associated with the development will provide for some on street parking . The site has sufficient parking on site to address the requirements of the development (as per the Norfolk County Zoning Bylaw).

As such, we would conclude that the proposed development on Grove Street will not have a significant impact on the traffic network in the area

I trust that this information will be sufficient to clear the applicable conditions of the planning on this site. If you require any additional information or clarification, please do not hesitate to contact this office.

Yours truly,

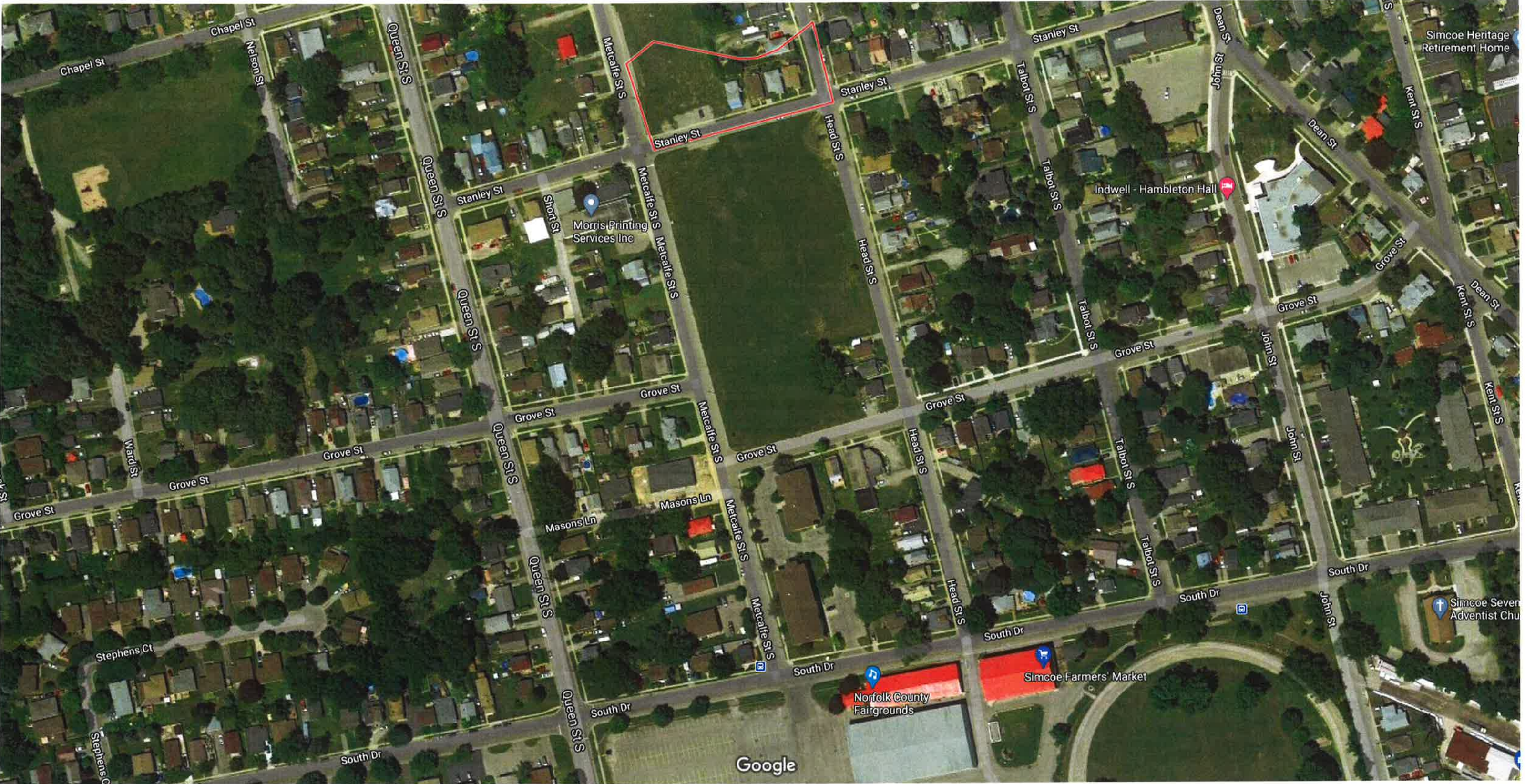
J.H. COHOON ENGINEERING LIMITED



R.W. Phillips, P.Eng.



c.c. Mike Quattrociocchi – Mayberry Homes



**FUNCTIONAL SERVICING REPORT
PROPOSED RESIDENTIAL DEVELOPMENT**

Grove Street and Metcalfe Street

**Simcoe, Ontario
Norfolk County**

Prepared By:

**J.H. Cohoon Engineering Limited
440 Hardy Road, Unit 1
Brantford, Ontario
N3T 5L8
Phone (519) 753-2656
Fax (519) 753-4263**

Job: 14316

February 2021

INTRODUCTION

The following Functional Servicing Report was prepared by J.H. Cohoon Engineering Limited for Mr. 'Mike Quattrociocchi of Mayberry Homes (Brantford) in support of an application for planning approvals for a proposed residential development that is proposed for the site located in downtown Simcoe, Ontario in Norfolk County.

The legal description of the subject lands is All of Lots 79 & 80, Part of Lots 69, 70m & 78, Parts of Railway Lot 2, All in Block 109, Registered Plan 182, Town of Simcoe, Norfolk County. A preliminary draft plan of subdivision is located within Appendix 'A' of this report.

The objective of this report is to document the servicing and grading strategy to be utilized for the site. Full services will be installed (i.e. sanitary, storm and water) within the development with connections to the existing municipal systems within the abutting rights-of-way are being proposed.

The site is presently an existing vacant land with the proposal to construct six (6) semidetached residential dwellings (Twelve (12) units total).

PROPOSED DEVELOPMENT CONCEPT

The proposed development comprises a total of 0.264 hectares. A key map illustrating the site location is provided in Figure 1.

The ultimate proposed development is illustrated on the site development (Grading and Servicing Plan) as prepared by J H Cohoon Engineering Limited. A copy of this development plan has been included within Appendix 'A' of this report. The preliminary servicing and grading have been included within Appendix 'B' of this report.

KEY PLAN



FIGURE 1
SITE LOCATION MAP

SANITARY SEWERS & APPURTENANCES

3.1 Design Flows

This particular site is proposed to be fully connected to the municipal sanitary sewer system that is located on Grove Street adjacent to the site in downtown Simcoe, Norfolk County. The proposed buildings and the applicable services are illustrated on the attached grading and servicing plan as prepared by J H Cohoon Engineering Limited (Drawing SP1 and GP1 which have been included within Appendix 'B' of this report). In accordance with the current Norfolk County requirements, the design flows have been submitted to the Norfolk County Development Services Department for the review of the conveyance systems

within the Town of Delhi (Norfolk County). The following information was provided to the Norfolk County for their use and consideration.

Sanitary Design Flows

Residential Component

12 Units Consists of 12 semi-detached units

2.75 person per unit

As per the requirements of the Norfolk County, the average daily flow is based upon 450 litres per person per day

$450 \times 2.75 \times 12 = 14,850$ litres per day

$= 0.172$ litres per second

Total Average Design Flow $= 0.172$ litres per second.

On the basis of the Harmon Peaking Factor, and a total population for this site being 33 persons, the peaking factor of 4.348 (Max 4.0) was applied resulting in a peak design flow for this building being 0.688 litres per second.

With the consideration of Infiltration on this site as follows:

Site Area $= 0.264$ hectares

Infiltration Rate $= 0.26$ litres per second per hectare

Infiltration Allowance
 $= 0.069$ litres per second

Summary of Results

Average Flow Rate (including Infiltration) $= 0.241$ litres per sec

Peak Flow Rate (including infiltration) $= 0.757$ litres per sec

The proposed and existing service connections have been illustrated on the preliminary servicing and grading plan as submitted being J H Cohoon Engineering Ltd drawing SP1 (Attached within Appendix 'B'). The preliminary sanitary sewer design sheets have been included within Appendix 'C' for this development.

3.2 Sanitary Outlet

The sanitary sewer system for the subject development will be connected into the existing sanitary sewers on Grove Street adjacent to the development. The analysis relating to the overall impact of this development on the receiving sanitary sewer system is being reviewed by Norfolk County Development Services Department.

WATERMAINS & APPURTENANCES

4.1 Design Flows

The peak design flow rate from the proposed development using current Norfolk County design standards. As with the wastewater, the estimated average flows have been detailed with the Sanitary Sewer Section of this report. (Section 3.1 above).

However, in this case the peaking factor of 2.25 has been utilized. The water demands have been updated to utilize a demand of 450 litres per person per day.

The summary of the water system demands can be summarized as follows:

Average Daily Flow Rate (Litres per second)	Peak Daily Flow Rate* (Litres per second)
0.172	0.387

As per the Norfolk County design criteria, a peaking factor of 2.25 was used for the calculation of the peak flow rate. The peak hourly flow rate is 0.688 litres per second on the basis of a peaking factor of 4.

The proposed fire protection to this development will be handled by the proposed fire hydrants to be located on the property.

The typical fire demand for single family and semi-detached units is 30 litres per second.

In the evaluation of the required fire flow for this development, the Water Supply for Public Fire Protection 1999, the required fire flow is based upon the characteristics of the building. The initial development is being considered as a 104.1 sq.m. residential building (one or two storey in height). The separation of the homes is such that the exposure distances are less than 3m.

The document requires that the floor area be utilized in the calculation of the required fire flows, and as such, the following calculations indicates that the floor area would be in the vicinity of 208.2 m². This will be utilized in the calculation of the fire flow required in accordance with the Fire Underwriters Survey 1999

Utilizing the Fire Underwriters Survey Document, our estimation of the required fire demand is as follows:

$$\text{Estimate of Fire Flow Required} = 220 * C * \text{SQRT}(A)$$

February 2021

Where C = Coefficient related to the type of construction

In this case, ordinary construction complete with combustible construction
(Combustible Construction = 1.5)

A = Total Area of the Building (As outlined above) of 208.2 sq.m

Flow = $220 * 1.5 * \text{SQRT}(208.2)$
= 4,761.6 litres per min

Rounded
= 5,000 litres per min

Modifications
Occupancy

= Low Hazard Fire Hazard -25%
= - 1,250 litres per min

Net Fire Demand

= 3,750 litres per min

Further Modifications

Automatic Sprinkler System -0%
= - 0 litres per min

Spatial Exposure – taken to abutting buildings or applicable property lines

Street + 0%
Left 0.76m +/- + 25 %
Right 0.76m +/- + 25 %
Rear 20.3m +/- + 10 %
Total + 50 %
= +1,875 litres per min

Total Fire Demand

= 5,625 litres per min
= 6,000 litres per min (Rounded)
= 100.0 litres per sec.

The proposed fire protection to this development will be handled by the proposed fire hydrants within the area.

The service sizes for this building have been illustrated on the attached plan.

GRADING / ROAD CONFIGURATION

Preliminary road grades have been established for the proposed development and are illustrated on the plans included within Appendix 'B' of this report. The minimum road grade that has been utilized on this development is consistent with the minimum 0.50% and maximum 6.0% road grades specified within the Norfolk County design standards. Individual lot grading patterns are illustrated on the attached plans with a minimum of slope of 1.5% and maximum slope of 4.0% within the development. The proposed grading is in accordance with good engineering practices.

UTILITIES


It is proposed that this development will be serviced with all normal municipally available utilities. Gas, Underground Hydro, Bell, and Cable TV utilities are available to service the proposed development within the municipal right-of-way (Grove Street right-of-way). The proposed development will include hydro distribution and private street lighting will be provided in accordance with the requirements of the Norfolk County. The preparation of the required utility plans will be undertaken during the detailed engineering design for this phase of this development.

CONCLUSIONS

The preceding sections of this report outline the preliminary servicing and grading requirements for the proposed development of the site located in the Town of Simcoe adjacent to the Grove Street right-of-way in Norfolk County. Based on the work completed to date, it may be concluded that the proposed development may be developed with full municipal services.

Report Prepared By:

J.H. COHOON ENGINEERING LIMITED


R. W. Phillips, P.Eng.



STORM SEWERS & APPURTENANCES

5.1 Storm Sewer System

In accordance with the Norfolk County, the proposed development will be serviced through the existing stormwater management facility located adjacent to the site. The site is located directly on Grove Street and drainage is directed into the existing infrastructure located on Grove Street. (The stormwater management report will be prepared on the basis of a final engineering design of this subdivision). The storm sewers are designed to the current Norfolk County standards, being designed for the 5-year storm event.

A stormwater management brief is required to ensure that the runoff associated with the re-development of this site, has been reduced to below the pre-development rate

With the re-development of this site and the inclusion of landscaped areas, we anticipate that the runoff from the site will be reduced to below the pre-development runoff rates from the property. The reduction in the runoff eliminates the need for additional stormwater management techniques (orifice plates and/or storage tanks, etc) being implemented on the property.

APPENDIX 'A'
PRELIMINARY DRAFT PLAN OF SUBDIVISION

DRAFT PLAN
OF
SUBDIVISION

ALL OF LOTS 79 & 80
PART OF LOTS 69, 70 & 78
PART OF RAILWAY LOT 2
ALL IN BLOCK 109
REGISTERED PLAN 182
TOWN OF SIMCOE
NORFOLK COUNTY




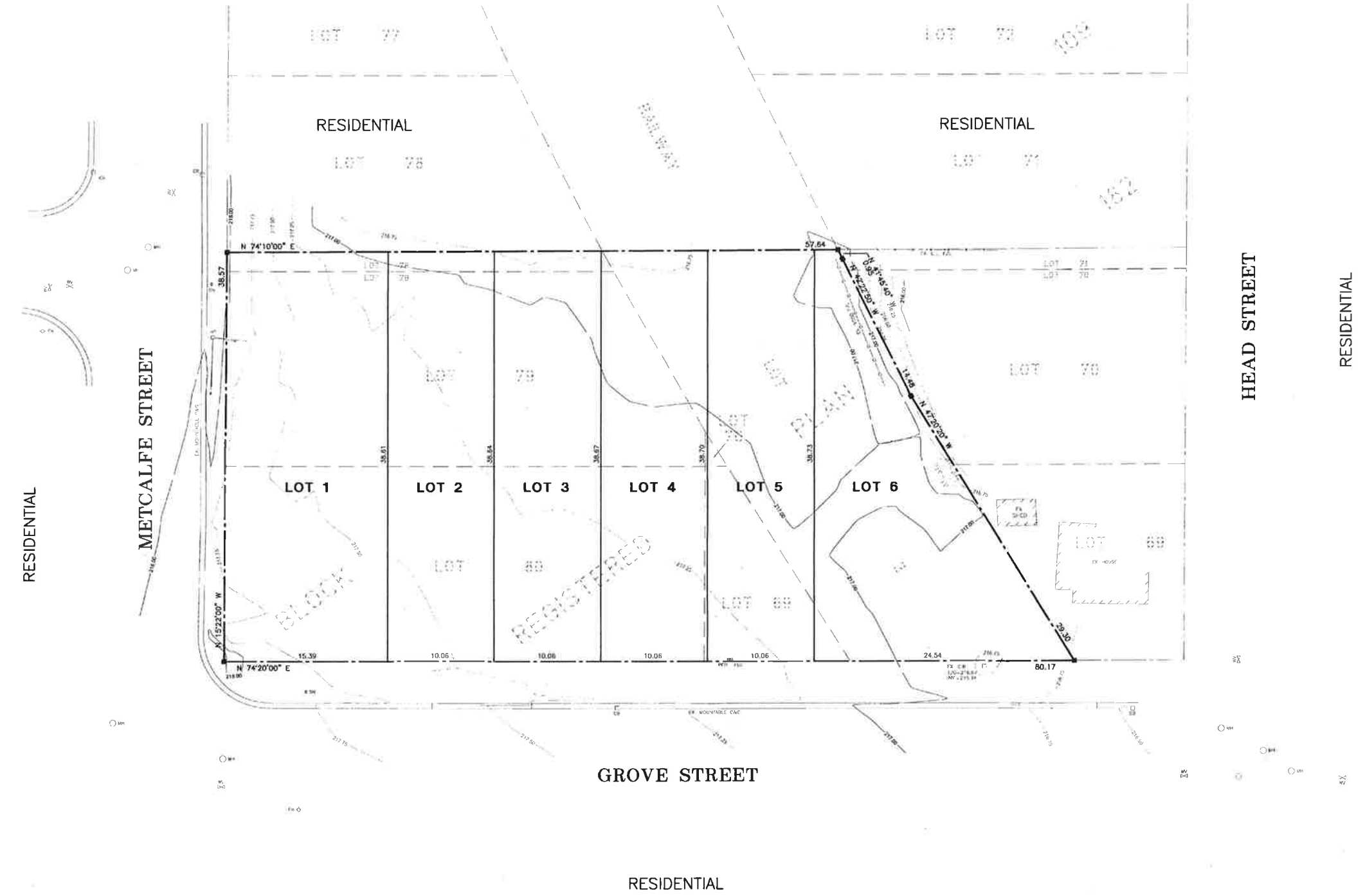
SURVEYOR'S CERTIFICATE
I HEREBY CERTIFY THAT THE BOUNDARIES OF THE LANDS TO BE SUBDIVIDED AS SHOWN ON THIS PLAN AND THEIR RELATIONSHIP TO THE ADJACENT LANDS ARE ACCURATELY AND CORRECTLY SHOWN.
DATE: Feb 16/2021
JIM JOHNSON, O.L.S.
WEST & RUUSKA LTD.

OWNER'S CERTIFICATE
I HEREBY AUTHORIZE J.H. COHOON ENGINEERING LTD. TO PREPARE AND SUBMIT THIS DRAFT PLAN OF SUBDIVISION TO THE CORPORATION OF NORFOLK COUNTY FOR APPROVAL.
DATE: _____ MAYBERRY HOMES

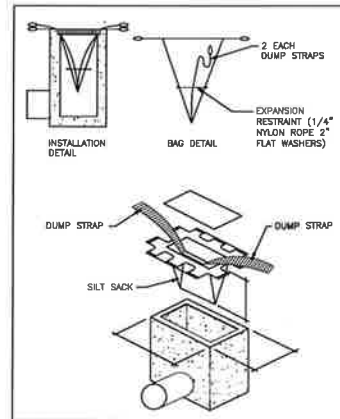
ADDITIONAL INFORMATION REQUIRED UNDER SECTION 51(17) OF THE PLANNING ACT
A - SEE PLAN
B - SEE PLAN
C - SEE PLAN
D - RESIDENTIAL (SEMI-DETACHED)
E - SEE PLAN
F - SEE PLAN
G - SEE PLAN
H - MUNICIPAL WATER
I - SAND AND GRAVEL
J - SEE PLAN
K - SANITARY & STORM SEWERS
L - NONE

AREA SCHEDULE
TOTAL AREA = 0.264 ha

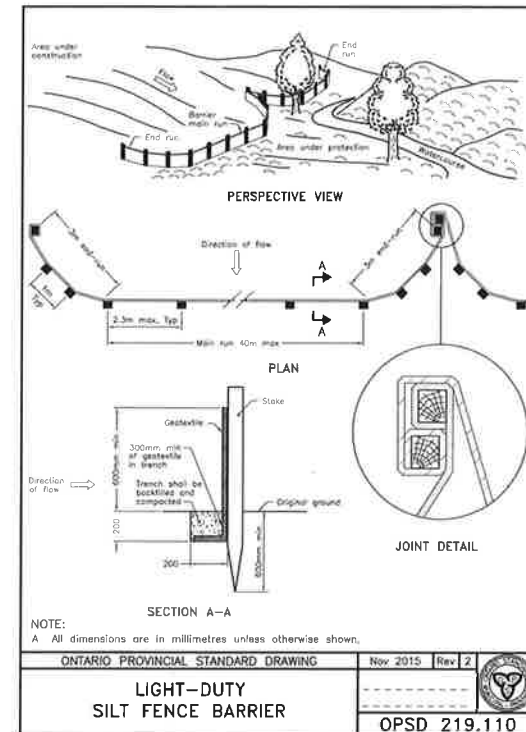
 J.H. COHOON ENGINEERING LIMITED CONSULTING ENGINEERS <small>440 HWY 30 ROAD - UNIT #1 BRIMFORD - CANADA - M3J 3J7 TEL: (905) 753-2050 FAX: (905) 753-4203 www.cohooneng.com</small>	DESIGN R.W.P.	PROJECT No.
	DRAWN K.P.B.	14316
	SCALE 1:200	DATE FEB. 9/21
	DATE FEB. 9/21	DP1



APPENDIX 'B'
PROPOSED GRADING AND SERVICING PLANS
(Prepared by J H Cohoon Engineering Limited)



SILT SACK DETAIL
N.T.S.



NOTE:
A All dimensions are in millimetres unless otherwise shown.

ONTARIO PROVINCIAL STANDARD DRAWING
LIGHT-DUTY
SILT FENCE BARRIER
Nov 2015 Rev 2
OPSD 219.110

THE POSITION OF POLE LINES, CONDUITS, WATERMANS, SEWERS AND OTHER UNDERGROUND AND ABOVEGROUND UTILITIES AND STRUCTURES ARE NOT NECESSARILY SHOWN ON THE CONTRACT DRAWINGS, AND WHERE SHOWN, THE ACCURACY OF THE POSITION OF SUCH UTILITIES AND STRUCTURES IS NOT GUARANTEED BEFORE COMMENCING WORK. THE CONTRACTOR SHALL FAMILIARIZE HIMSELF OF THE EXACT LOCATION OF ALL SUCH UTILITIES AND STRUCTURES AND SHALL ASSUME ALL LIABILITY FOR DAMAGE TO THEM.

- LEGEND:
- EXISTING ELEVATIONS
 - PROPOSED ELEVATIONS
 - PROPOSED ELEVATIONS (ADJUTING DESIGN)
 - PROPOSED SWALE ELEVATIONS
 - PROPOSED SWALE ELEVATIONS (DESIGN)
 - PROPOSED SWALE
 - GENERAL DRAINAGE
 - PROPOSED SILT FENCE
 - SILT SACK AS SHOWN

- NOTES:
- ALL ELEVATIONS SHOWN ARE METRIC.
 - BUILDER/OWNER TO VERIFY COMPLIANCE WITH ZONING BYLAWS (e.g. SIDEYARDS, SETBACKS, REARWARDS ETC.)
 - WHERE ONLY ONE ELEVATION IS SHOWN, EXISTING AND PROPOSED ELEVATIONS ARE THE SAME.
 - THE SLOTTING & EROSION CONTROL (SEC) MEASURES ILLUSTRATED ON THIS PLAN ARE CONSIDERED TO BE THE MINIMUM REQUIREMENT. SITE CONDITIONS MAY REQUIRE ADDITIONAL MEASURES WHICH WILL BE IDENTIFIED BY THE ENGINEER DURING CONSTRUCTION.
 - ALL SEC MEASURES ARE TO BE IN PLACE PRIOR TO COMMENCEMENT OF CONSTRUCTION.
 - OWNER/CONTRACTOR TO MAINTAIN EROSION CONTROL MEASURES THROUGHOUT SITE UNTIL A COMPLETE GRASS/VEGETATION COVER IS ACHIEVED.
 - ONLY AT THE DIRECTION OF THE ENGINEER ARE THE SEC MEASURES TO BE REMOVED.
 - ALL EXPOSED AREAS NOT SUBJECT TO ACTIVE CONSTRUCTION WITHIN 30 DAYS ARE TO BE REVEGETATED AS PER O.P.S.S. 972 IMMEDIATELY UPON COMPLETION OF AREA GRADING.
 - CONTRACTOR TO PROVIDE SILT FENCE AROUND PERIMETER OF ALL ON SITE STOCKPILES.
 - CONTRACTOR TO PROVIDE SILT SACKS ON TOP OF ALL EXISTING AND PROPOSED STORM STRUCTURES WITHIN THE INFLUENCE OF RUNOFF DURING CONSTRUCTION UNTIL ADEQUATE VEGETATIVE COVER IS ACHIEVED.

T.B.M. No. 1 ELEV. = 218.94m (GEO)
TOP NUT OF FIRE HYDRANT AT THE INTERSECTION OF METCALFE AND GROVE STREETS AS SHOWN.

T.B.M. No. 2 ELEV. = 216.82m (GEO)
TOP NUT OF FIRE HYDRANT AT THE INTERSECTION OF HEAD AND GROVE STREETS AS SHOWN.

NO. REVISION DATE (MM/DD/YYYY) BY

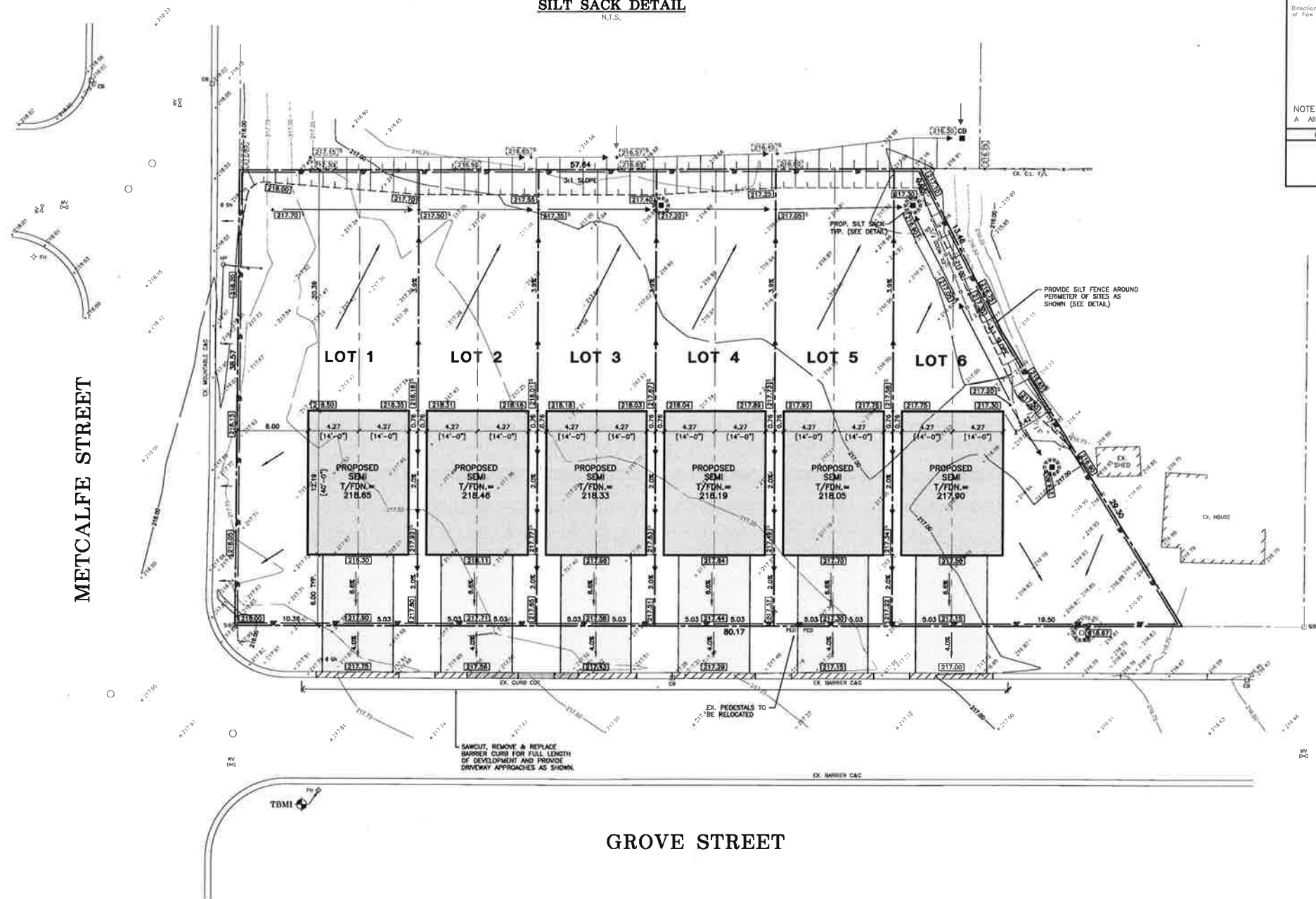
J.H. COHOON
ENGINEERING
LIMITED
CONSULTING ENGINEERS
440 HARDY ROAD, UNIT #1, BRANTFORD - ONTARIO, N3T 5L8
TEL: (519) 753-2656 FAX: (519) 753-4283 www.cohooneg.com

PROJECT:
PROPOSED
RESIDENTIAL
DEVELOPMENT
GROVE STREET, SIMCOE
NORFOLK COUNTY

CLIENT:
MAYBERRY HOMES

GRADING & SILTATION AND
EROSION CONTROL PLAN

DESIGN:	R.W.P.	SCALE:	1:200
DRAWN:	K.P.B.	JOB No:	
CHECKED:	R.W.P.		14316
SHEET:	1 of 2	DWG No:	
DATE:	FEB. 26/21		14316-1



SANITARY SEWERS:

1. SANITARY & STORM SEWERS & RELATED APPURTENANCES SHALL BE DESIGNED AND CONSTRUCTED SO AS TO COMPLY WITH APPLICABLE LAW, TO BE CONSISTENT WITH THE COUNTY OF NORFOLK DEVELOPMENT AND ENGINEERING STANDARDS AND IN ACCORDANCE WITH CURRENT GUIDELINES, CODES, REGULATIONS, BEST PRACTICES AND STANDARDS PRESCRIBED BY THE COUNTY.
2. COVER AND BEDDING MATERIAL FOR PVC PIPE AS PER OPSD 802.010 TYPE 2 TRENCH BEDDING SHALL BE GRANULAR 'A' MATERIAL UNLESS OTHERWISE INDICATED.
3. PVC PIPE WILL REQUIRE SPECIAL CONSTRUCTION PROCEDURES FOR LEAKAGE AND TESTING, PIPE DEFLECTION, ETC.
4. ALTERNATE MATERIALS MAY BE ACCEPTABLE, PROVIDED APPROVAL HAS FIRST BEEN OBTAINED FROM THE COUNTY OF NORFOLK AND ENGINEER IN WRITING.
5. ALL SEWER AND CULVERT INSTALLATIONS TO CONFORM WITH OPSD 802.031 TYPE 3 SOIL.
6. PRIVATE SANITARY DRAINS TO 125mm PVC DR28 PIPE AND HAVE A MIN. GRADE OF 2.0%, AS PER NORFOLK COUNTY DESIGN CRITERIA.
7. A 30x89mm x 2.0m LONG MARKER IS TO BE PLACED FROM THE CAPPED LATERAL AND EXTEND 1.0m ABOVE GROUND AND PAINTED GREEN FOR SANITARY AND WHITE FOR STORM.
8. BEDDING FOR PRIVATE SANITARY & STORM DRAINS AS PER OPSD 1008.02 TYPE 2 TRENCH WITH GRANULAR 'A' BEDDING AND COVER MATERIAL.
9. MINIMUM FALL FOR PRIVATE SANITARY & STORM DRAINS TO BE 2.0%

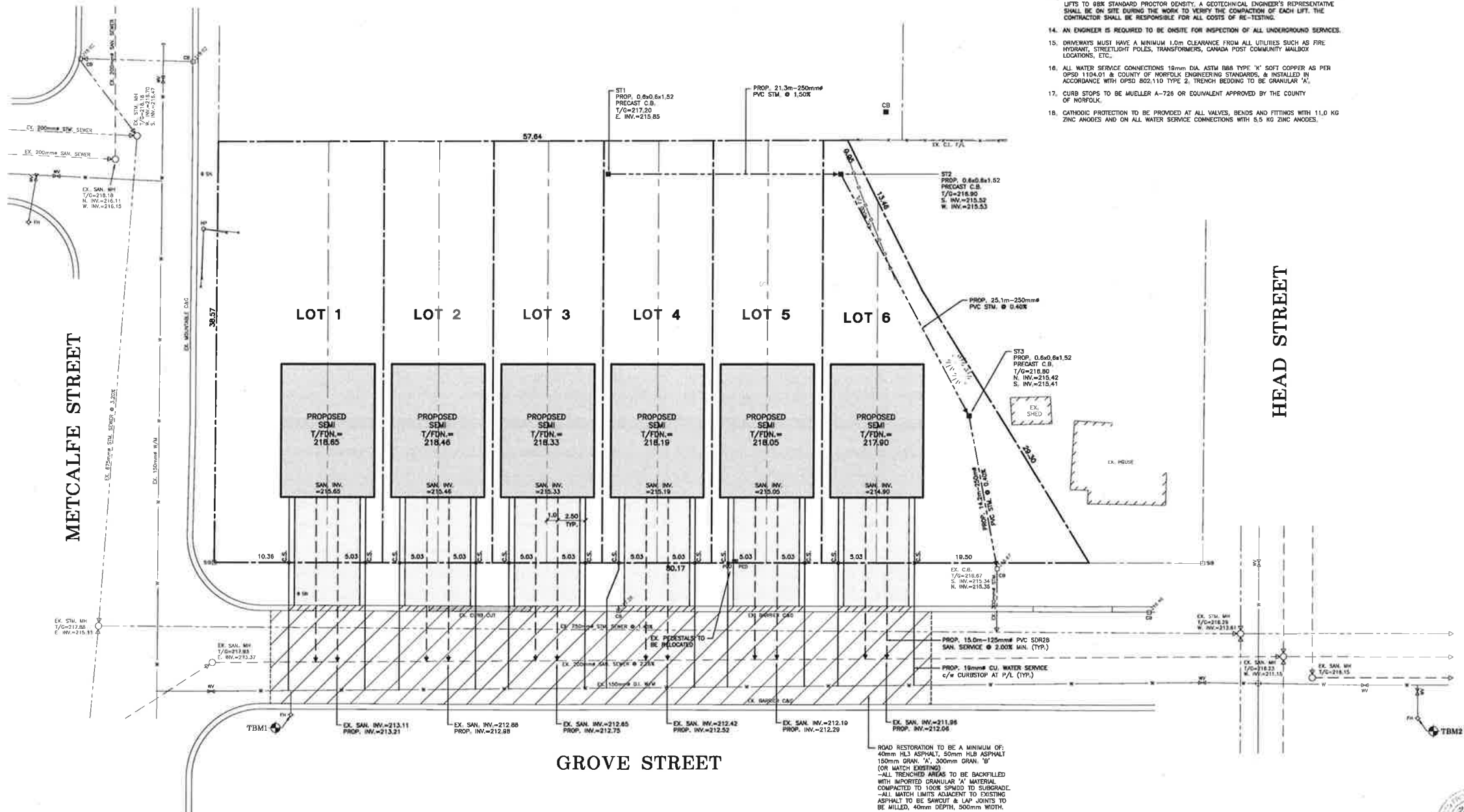
GENERAL NOTES:

1. CONSTRUCTION OF SEWERS, WATERMANS AND RELATED APPURTENANCES SHALL BE UNDERTAKEN IN ACCORDANCE WITH THE CURRENT STANDARD DRAWINGS OF THE COUNTY OF NORFOLK, AND THE ONTARIO PROVINCIAL STANDARDS DRAWINGS (OPSD). THE COUNTY OF NORFOLK DRAWINGS SHALL TAKE PRECEDENCE OVER THE OPSD DRAWINGS.
2. INFORMATION REGARDING ANY EXISTING SERVICES AND/OR UTILITIES SHOWN ON THE APPROVED SET OF CONSTRUCTION DRAWINGS IS FURNISHED AS THE BEST AVAILABLE INFORMATION. THE CONTRACTOR SHALL INTERPRET THIS INFORMATION AS THEY SEE FIT WITH THE UNDERSTANDING THAT THE OWNER AND HIS AGENTS DISCLAIM ALL RESPONSIBILITY FOR ITS ACCURACY AND/OR SUFFICIENCY.
3. ALL DIMENSIONS SHALL BE CHECKED AND VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO ANY CONSTRUCTION AND HE SHALL REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ENGINEER.
4. RELOCATION OF EXISTING SERVICES AND/OR UTILITIES SHALL BE CONSTRUCTED AS SHOWN ON THE DRAWINGS OR AS DIRECTED BY THE ENGINEER.
5. THE CONTRACTOR SHALL OBTAIN ALL PERMITS FOR CONSTRUCTION.
6. FOR ALL SEWERS AND WATERMAN IN FILL SECTIONS, THE COMPACTION SHALL BE VERIFIED PRIOR TO LAYING OF PIPE.
7. NO SUBSTITUTIONS WILL BE ALLOWED WITHOUT WRITTEN APPROVAL FROM THE COUNTY OF NORFOLK OR THE ENGINEER.
8. NO BLASTING WILL BE PERMITTED.
9. ALL EXCAVATIONS TO BE BACKFILLED WITH SELECT NATIVE MATERIAL, APPROVED BY THE ENGINEER, TO 95% S.P.D.
10. THE DEVELOPER AND/OR CONTRACTOR IS RESPONSIBLE FOR INSTALLING AND MAINTAINING (UNTIL ROAD CONSTRUCTION IS FINISHED) SILT CONTROL DEVICES AS SHOWN ON THE DRAWINGS AND AS DIRECTED BY THE ENGINEER.
11. ALL WORKS SHALL BE DESIGNED AND CONSTRUCTED SO AS TO COMPLY WITH APPLICABLE LAW, TO BE CONSISTENT WITH THE COUNTY OF NORFOLK DEVELOPMENT & ENGINEERING STANDARDS AND IN ACCORDANCE WITH CURRENT GUIDELINES, CODES, REGULATIONS AND STANDARDS PRESCRIBED BY THE COUNTY.
12. ALL BOULEVARD AREAS TO BE RESTORED WITH #1 NURSERY SOO ON A MINIMUM 100mm OF SELECT TOPSOIL.
13. ALL TRENCH BACKFILL UNDER EXISTING ROADWAYS SHALL BE COMPACTED IN MINIMUM 230mm LIFTS TO PER STANDARD PROCTOR DENSITY, A GEOTECHNICAL ENGINEER'S REPRESENTATIVE SHALL BE ON SITE DURING THE WORK TO VERIFY THE COMPACTION OF EACH LIFT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS OF RE-TESTING.
14. AN ENGINEER IS REQUIRED TO BE ONSITE FOR INSPECTION OF ALL UNDERGROUND SERVICES.
15. DRIVEWAYS MUST HAVE A MINIMUM 1.0m CLEARANCE FROM ALL UTILITIES SUCH AS FIRE HYDRANT, STREETLIGHT POLES, TRANSFORMERS, CANADA POST COMMUNITY MAILBOX LOCATIONS, ETC.
16. ALL WATER SERVICE CONNECTIONS 10mm DIA. ASTM B88 TYPE 'K' SOFT COPPER AS PER OPSD 1104.01 & COUNTY OF NORFOLK ENGINEERING STANDARDS, & INSTALLED IN ACCORDANCE WITH OPSD 802.110 TYPE 2, TRENCH BEDDING TO BE GRANULAR 'A'.
17. CURB STOPS TO BE MUELLER A-728 OR EQUIVALENT APPROVED BY THE COUNTY OF NORFOLK.
18. CATHODIC PROTECTION TO BE PROVIDED AT ALL VALVES, BENDS AND FITTINGS WITH 11.0 KG ZINC ANODES AND ON ALL WATER SERVICE CONNECTIONS WITH 5.5 KG ZINC ANODES.



LEGEND:

EX. 50mm-300mm STW @ 0.5%	EXIST. STORM SEWER SYSTEM
EX. 60mm-200mm SAN @ 0.4%	EXIST. SANITARY SEWER SYSTEM
—	EXISTING WATERMAIN
—x—	EXISTING CHAIN LINK FENCE
—o—o—o—	EXISTING WOOD FENCE
○ S1	EXISTING SANITARY MANHOLE
○ ST1	EXISTING STORM MANHOLE
□ CB	EXISTING CATCHBASIN
BR FID	EXISTING BELL PEDESTAL
D+ WY	EXIST. WATER VALVE
◇ FH	EXIST. FIRE HYDRANT
○ HP	EXISTING HYDRO POLE
⊕ TBM	EXISTING TEMPORARY BENCHMARK
8 SN	EXISTING SIGN
—	PROPOSED SANITARY SERVICE
—	PROPOSED WATER SERVICE



T.B.M. No. 1 ELEV. = 218.94m (GEO)
TOP NUT OF FIRE HYDRANT AT THE INTERSECTION OF METCALFE AND GROVE STREETS AS SHOWN.

T.B.M. No. 2 ELEV. = 216.82m (GEO)
TOP NUT OF FIRE HYDRANT AT THE INTERSECTION OF HEAD AND GROVE STREETS AS SHOWN.

NO.	REVISION	DATE (MM/DD/YY)	BY

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PROJECT:
**PROPOSED
RESIDENTIAL
DEVELOPMENT**
GROVE STREET, SIMCOE
NORFOLK COUNTY

CLIENT:
MAYBERRY HOMES

SERVICING PLAN

DESIGN:	R.W.P.	SCALE:	1:200
DRAWN:	K.P.B.	JOB No:	14316
CHECKED:	R.W.P.	DWG. No:	14316-2
SHEET:	2 of 2		
DATE:	FEB. 26/21		

LEGEND

■■■■■ SANITARY DRAINAGE BOUNDARY

1.20
10

← SANITARY DRAINAGE AREA IN HECTARES

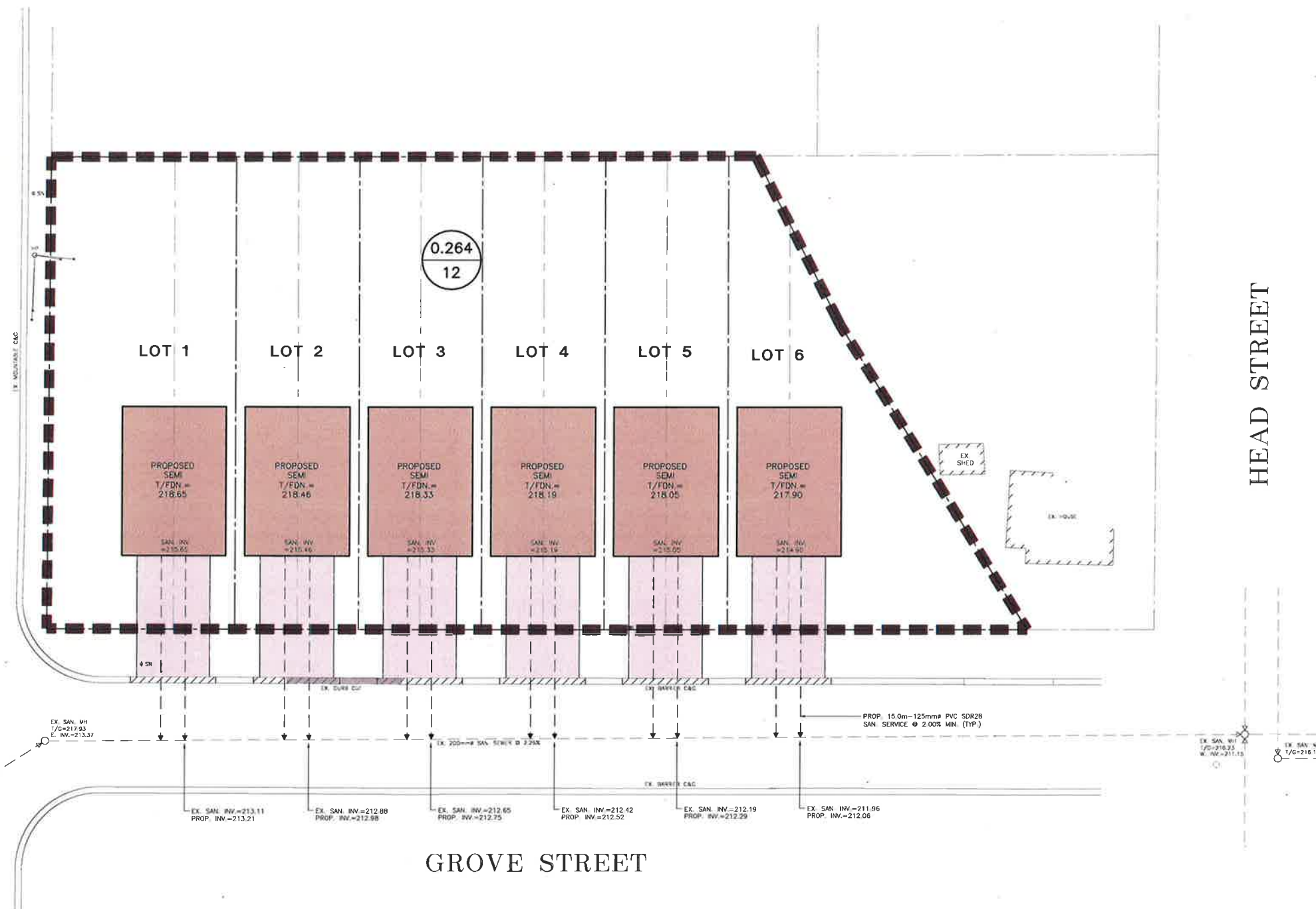
← NUMBER OF UNITS



METCALFE STREET

HEAD STREET

GROVE STREET



SANITARY DRAINAGE AREAS

PROPOSED RESIDENTIAL DEVELOPMENT
GROVE STREET — NORFOLK COUNTY



J.H. COHOON ENGINEERING LIMITED
CONSULTING ENGINEERS
BRANTFORD

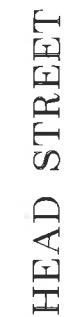
CLIENT: MAYBERRY HOMES
SCALE: 1:400

JOB: 14316

■■■■



← STORM AREA IN HECTARES



PROPOSED RESIDENTIAL DEVELOPMENT
GROVE STREET — NORFOLK COUNTY



JOB: 14316

STORM SEWER DESIGN

Designed by: R. W. Phillips, P.Eng.

Company: J.H. Cohoon Engineering Limited

Date : "February 2021

Proposed Residential Development
Grove Street at Metcalfe, Simcoe Ontario

$$I = A / [(B + T_c)^c]$$

A = 771.90

B = 6.24

$$n = 0.013$$

C= 0.780

[illegible]

Designed by: R. W. Phillips, P.Eng.	STORM SEWER DESIGN Proposed Residential Development Grove Street at Metcalfe, Simcoe Ontario	$I = A / [(B + T_c)^6]$	A = 771.90
Company: J.H. Cohoon Engineering Limited		n = 0.013	B = 6.24
Date: *February 2021			c = 0.780

[illegible]

[illegible]

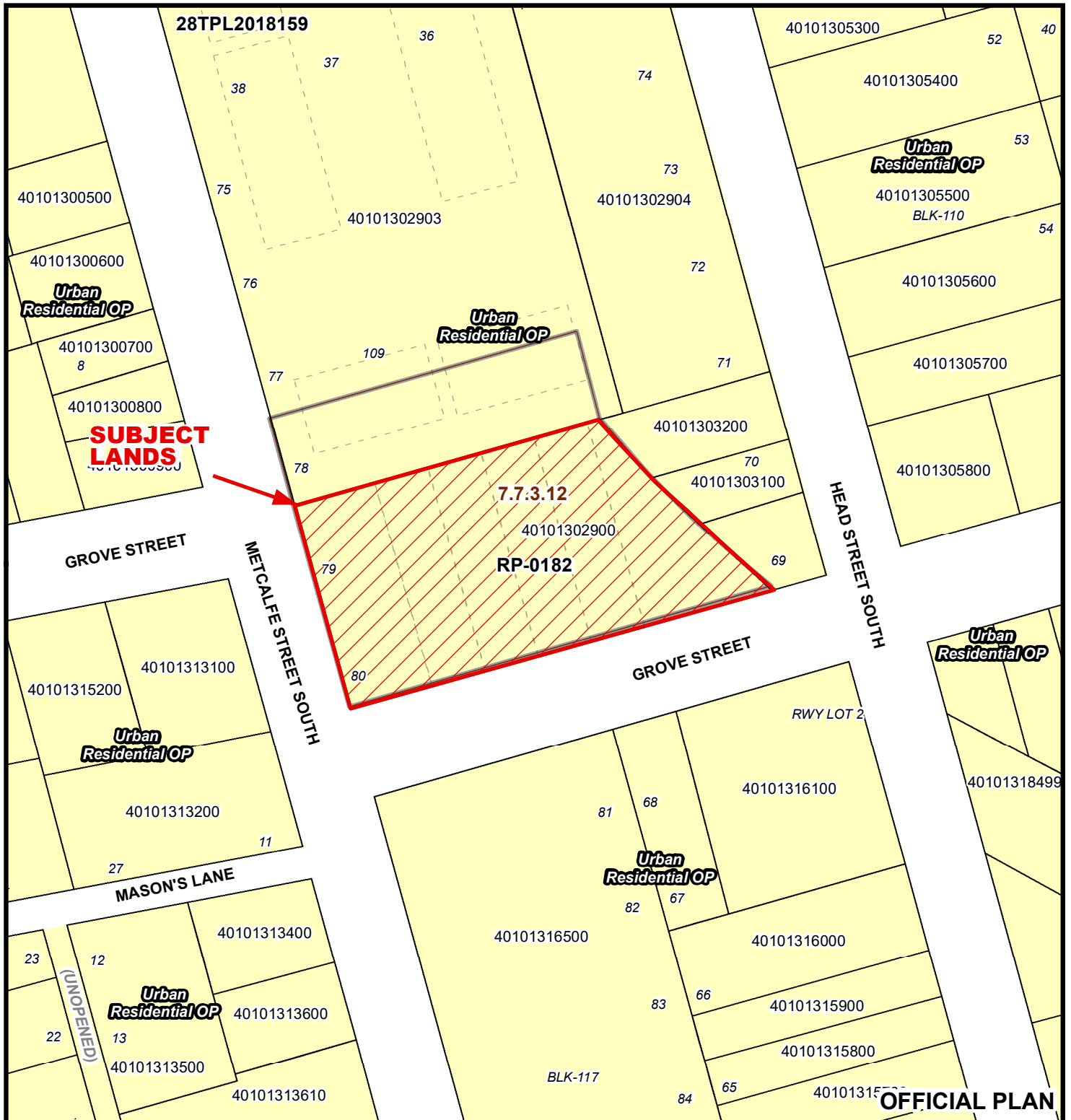
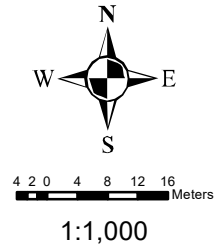
Company:	J.H. Cohoon Engineering Limited	Proposed Residential Development	B =	6.24
Date :	*February 2021	Grove Street at Metcalfe, Simcoe Ontario	n =	0.013
			c=	0.780

[illegible]

MAP 1

File Number: 28TPL2021067 & ZNPL2021064

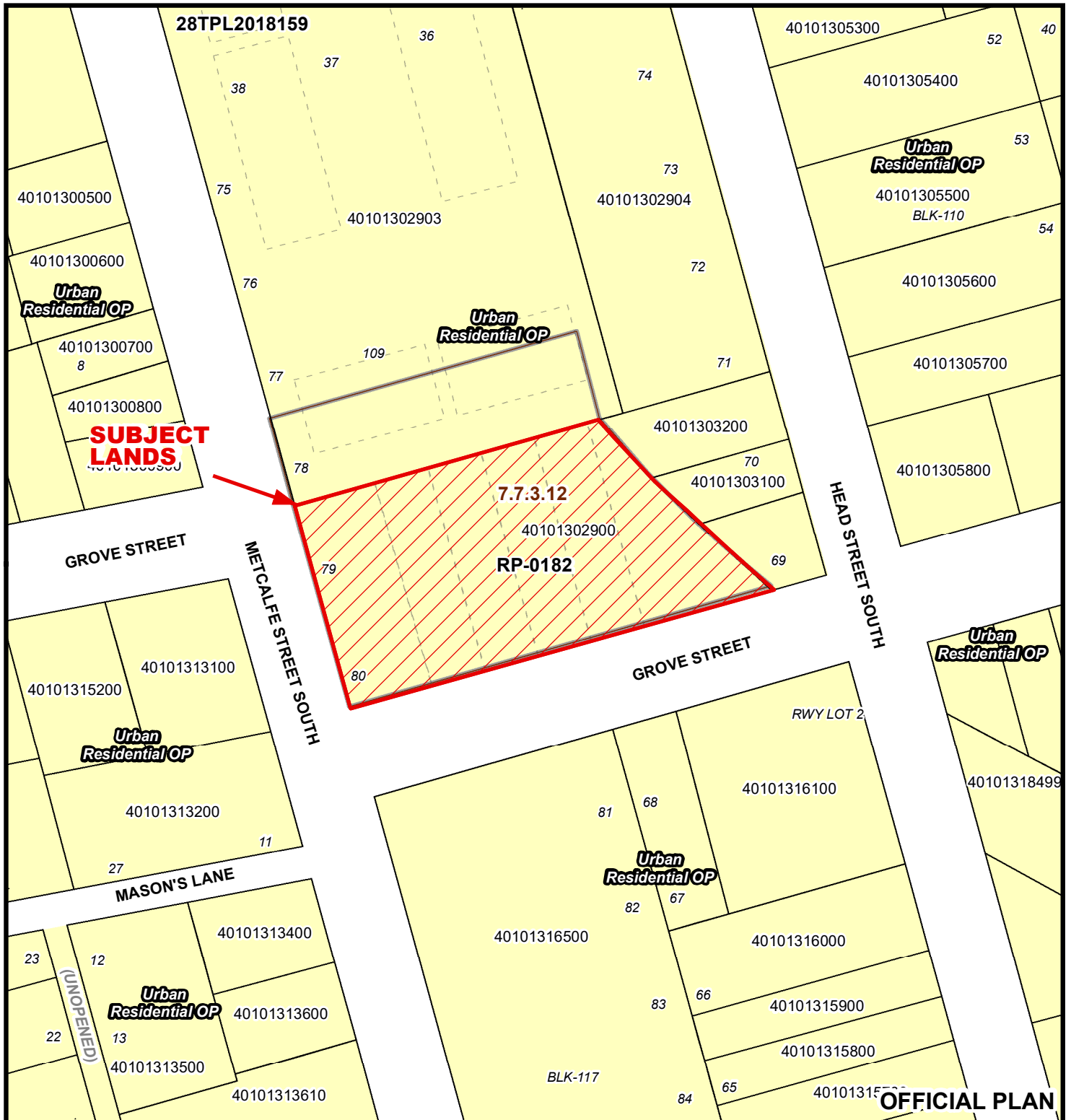
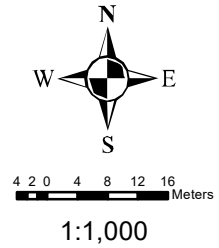
Urban Area of SIMCOE



MAP 2

File Number: 28TPL2021067 & ZNPL2021064

Urban Area of SIMCOE



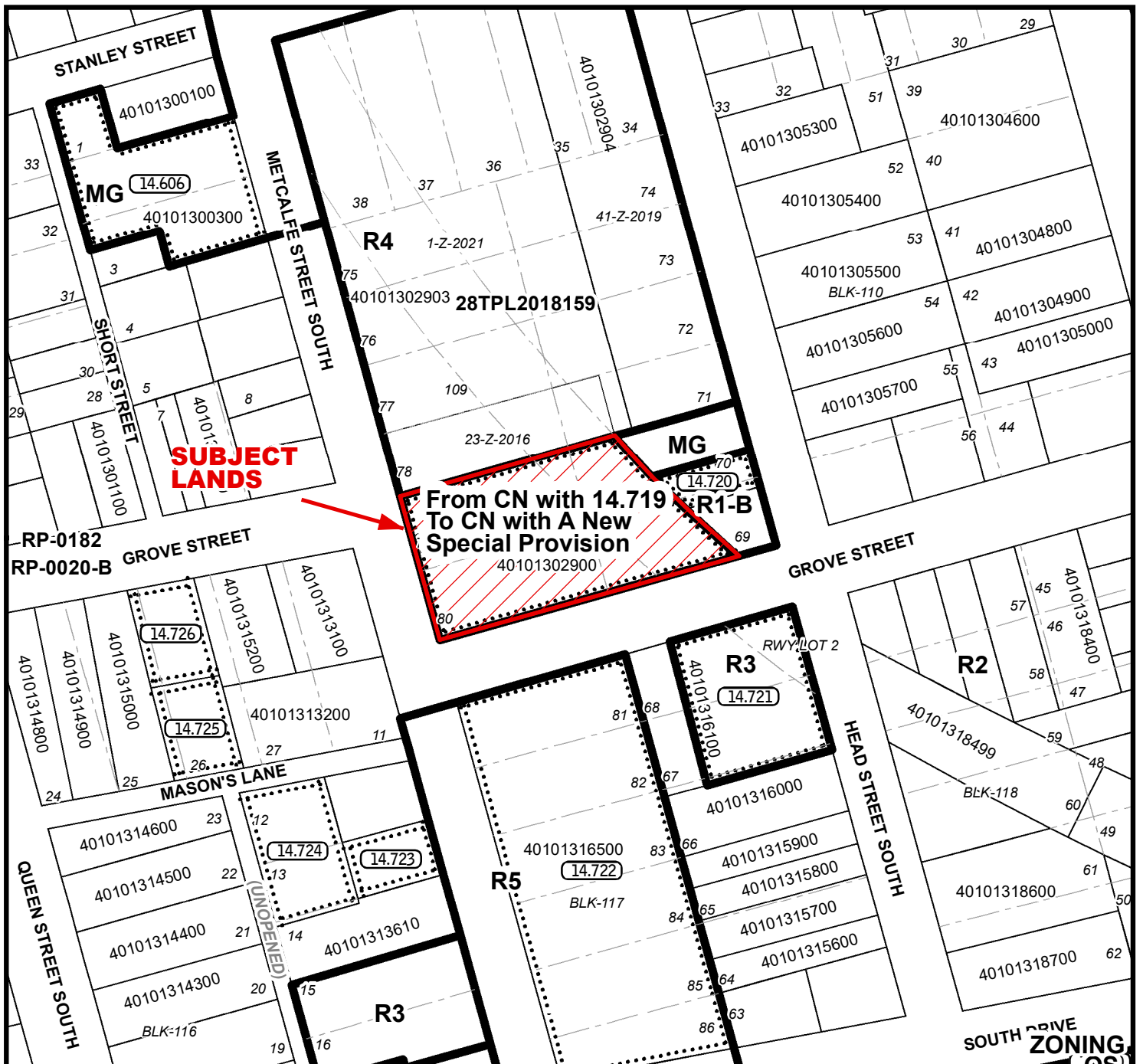
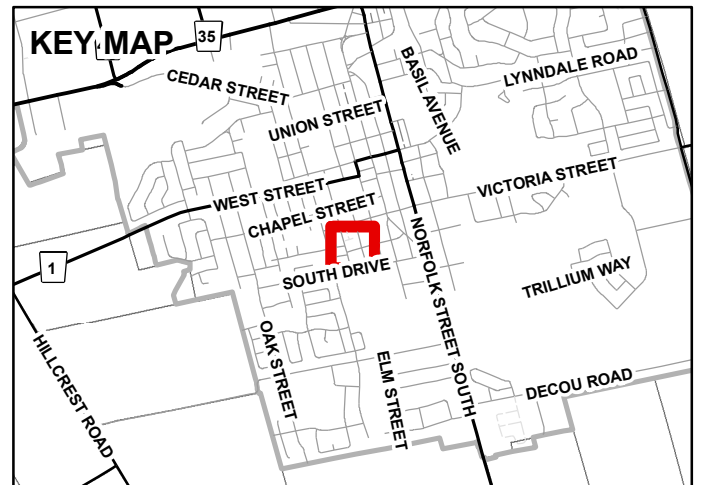
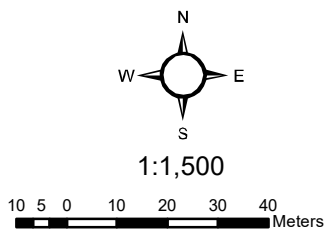
OFFICIAL PLAN

2021-05-10

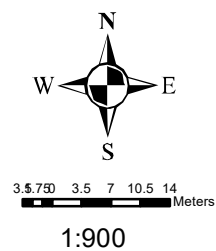
MAP 2

File Number: 28TPL2021067 & ZNPL2021064

Urban Area of
SIMCOE



Urban Area of SIMCOE



MAP 4

File Number: 28TPL2021067 & ZNPL2021064

Urban Area of SIMCOE



2 1 0 2 4 6 8 Meters

1:550

